



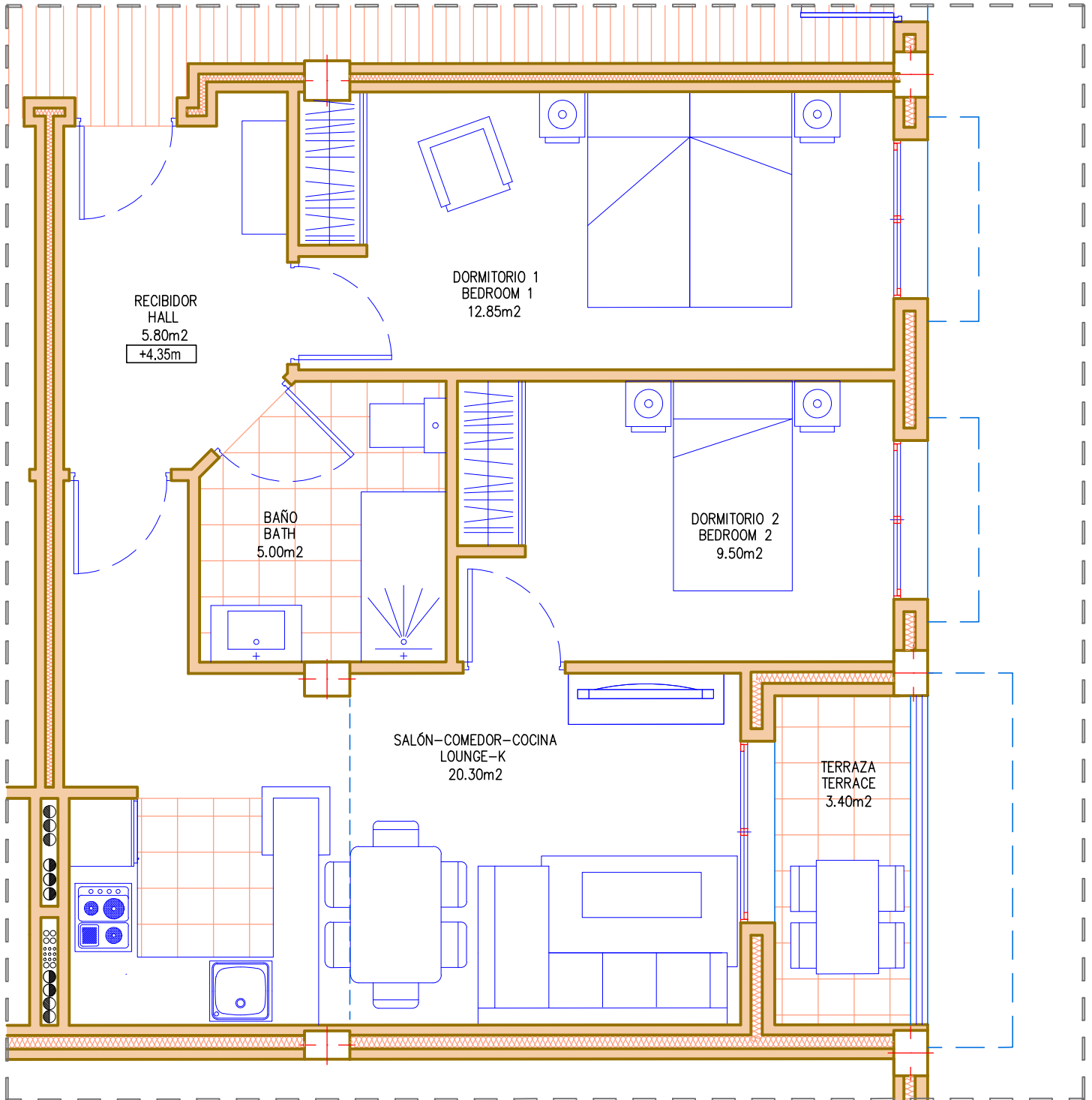
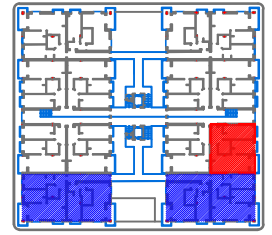
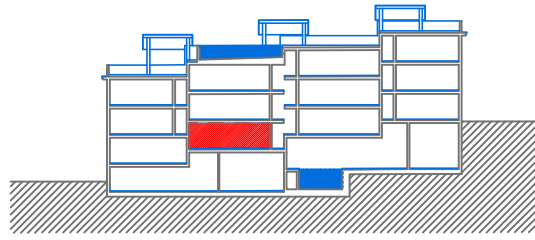
# APARTAMENTOS

44 Apartamentos con Garaje y Zonas Comunes  
Calle Cordoba/ Calle Sevilla S/N  
04639 Turre -ALMERÍA-España



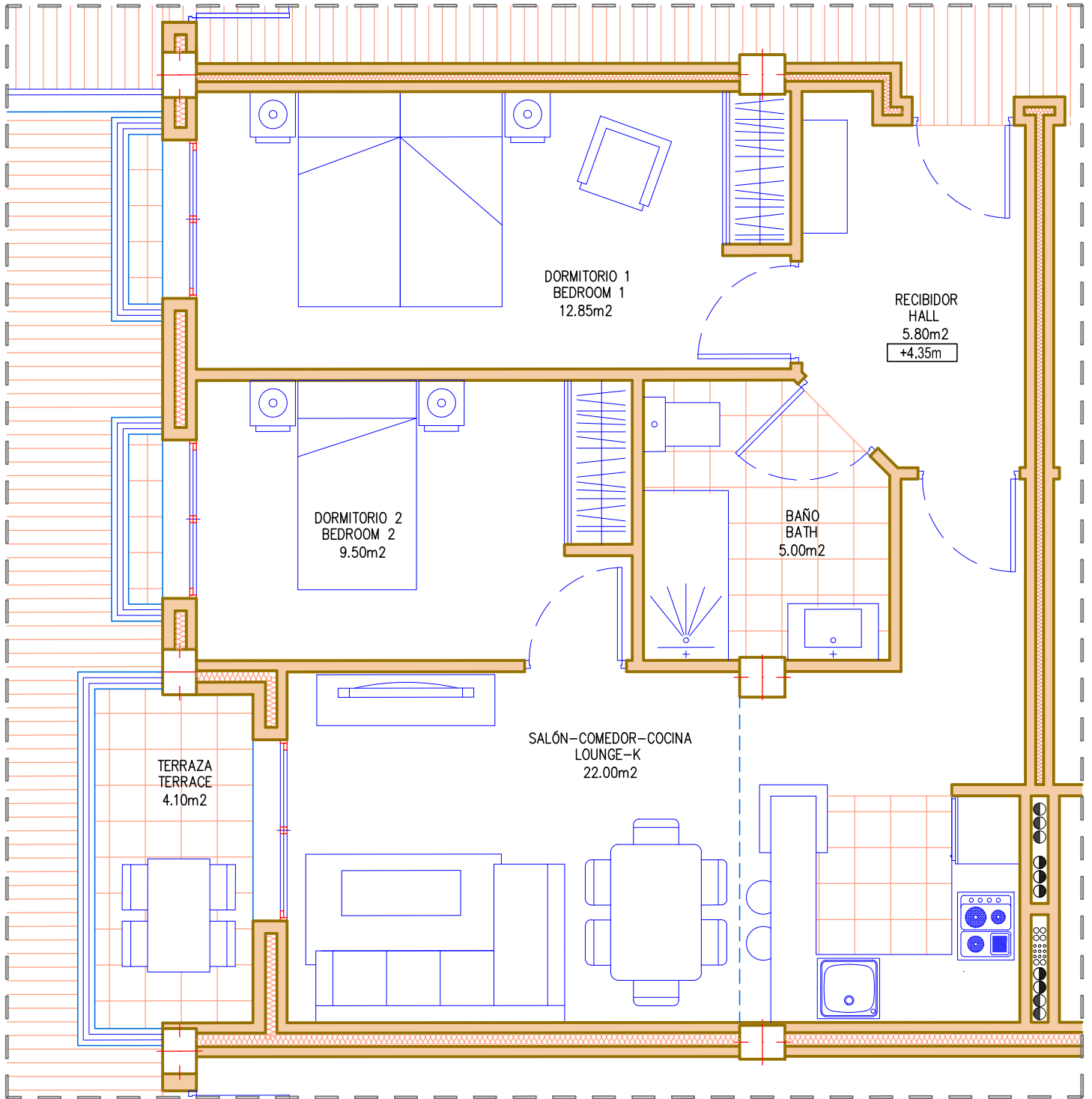
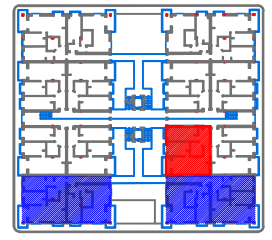
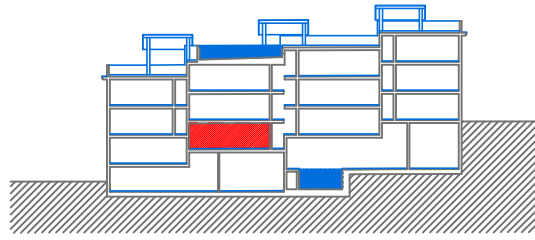
# APATAMENTO Nº 1

SUP. CONSTRUIDA\_ 65.50m<sup>2</sup>  
SUP. UTIL\_ 53.45m<sup>2</sup>



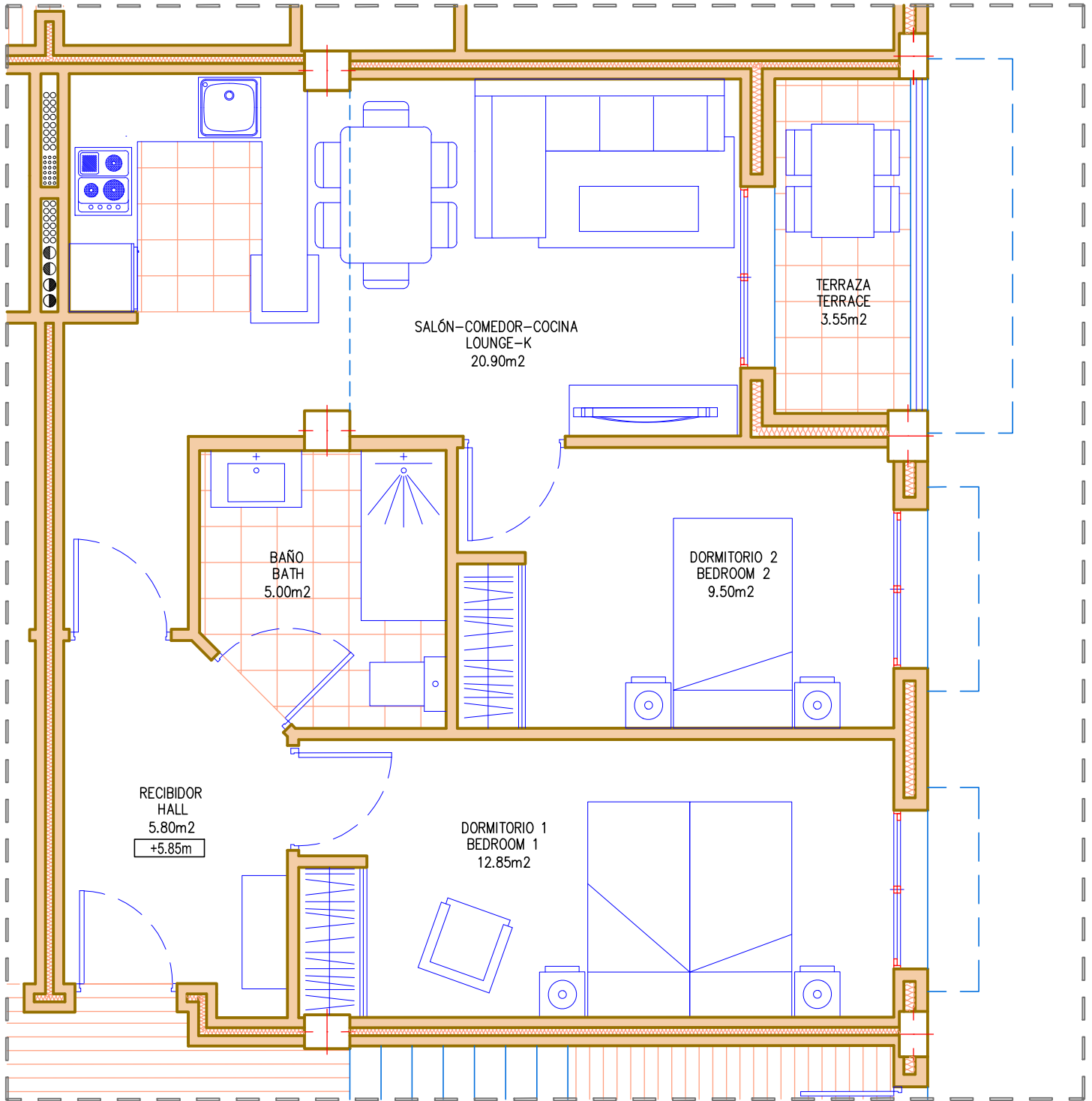
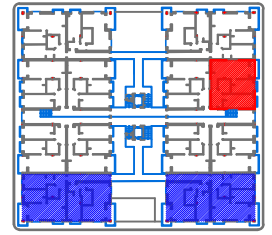
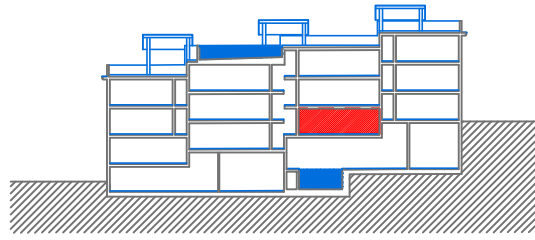
# APATAMENTO Nº 2

SUP. CONSTRUIDA\_ 65.50m<sup>2</sup>  
SUP. UTIL\_ 55.15m<sup>2</sup>



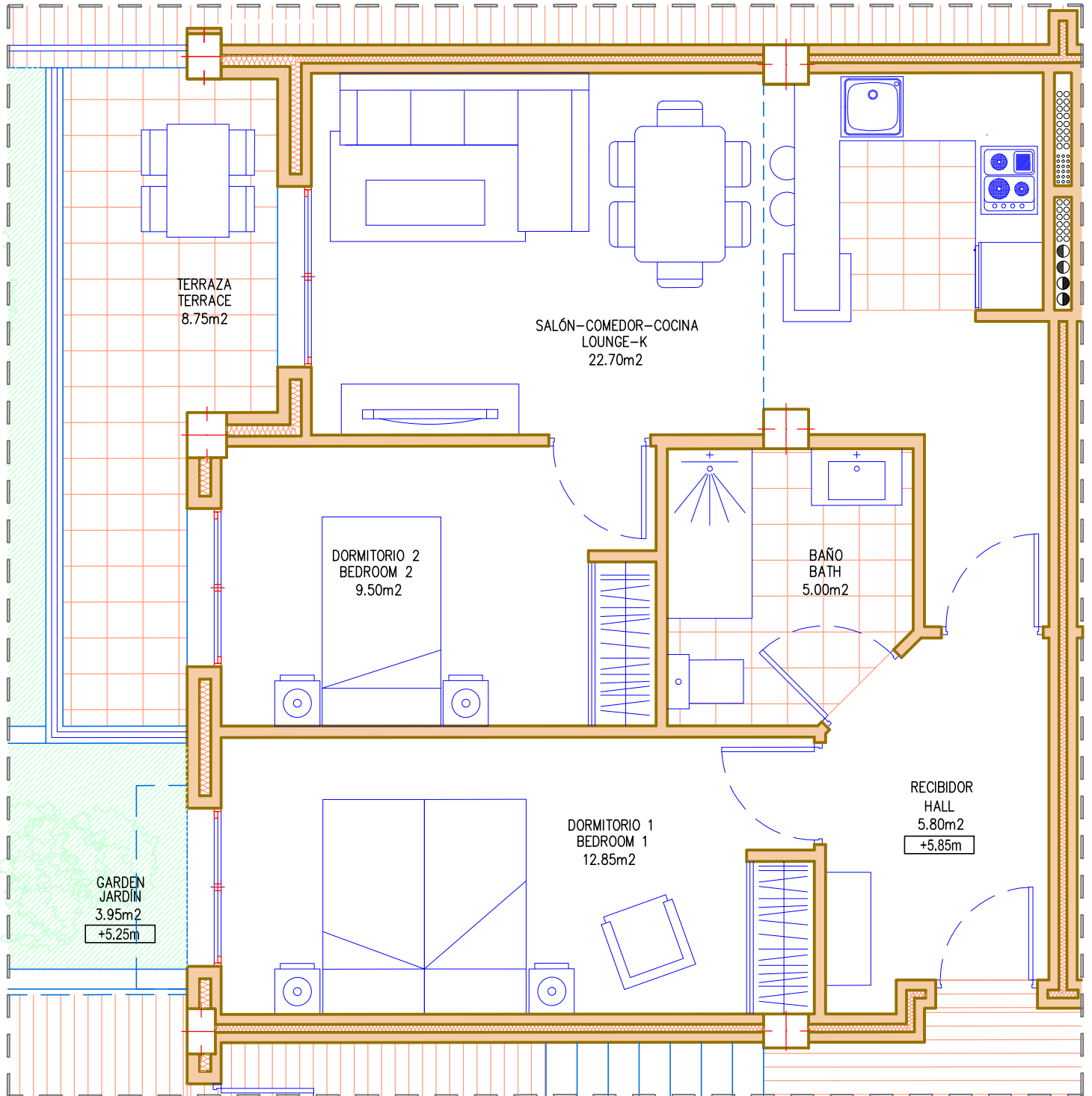
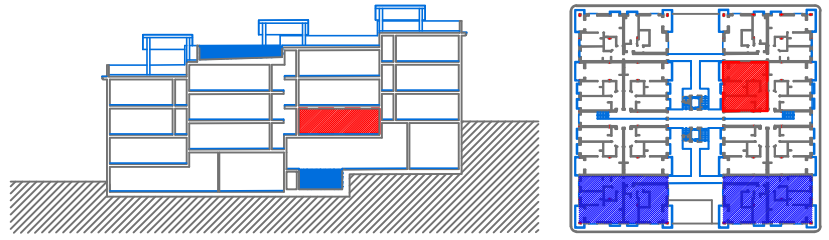
# APATAMENTO N° 3

SUP. CONSTRUIDA\_ 67.05m<sup>2</sup>  
SUP. UTIL\_ 54.05m<sup>2</sup>



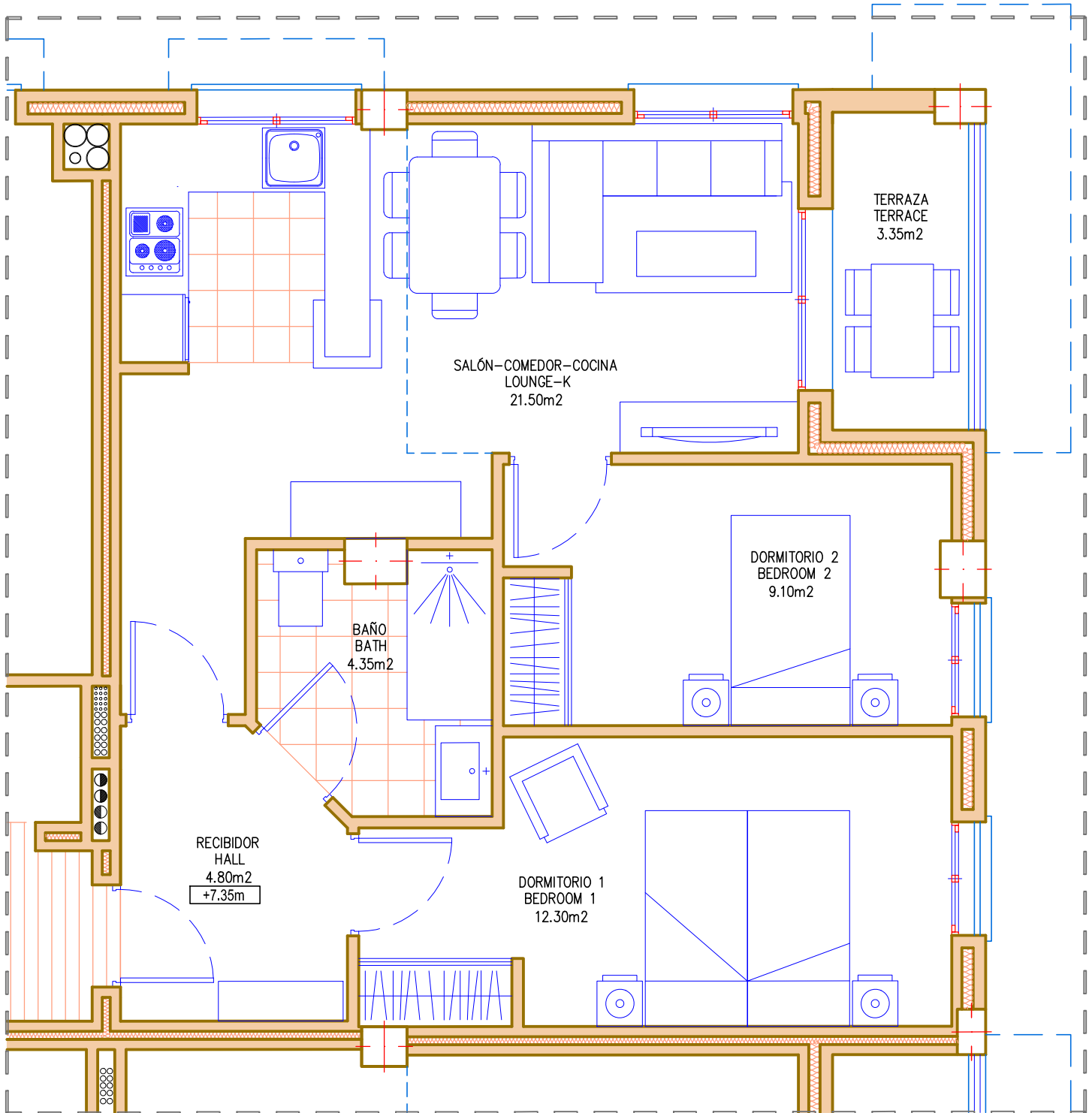
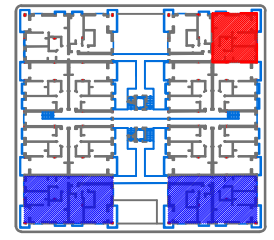
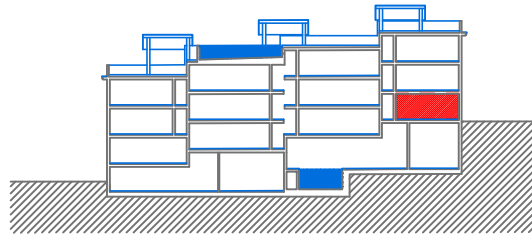
# APATAMENTO N° 4

SUP. CONSTRUIDA\_ 68.00m<sup>2</sup>  
SUP. UTIL\_ 55.85m<sup>2</sup>



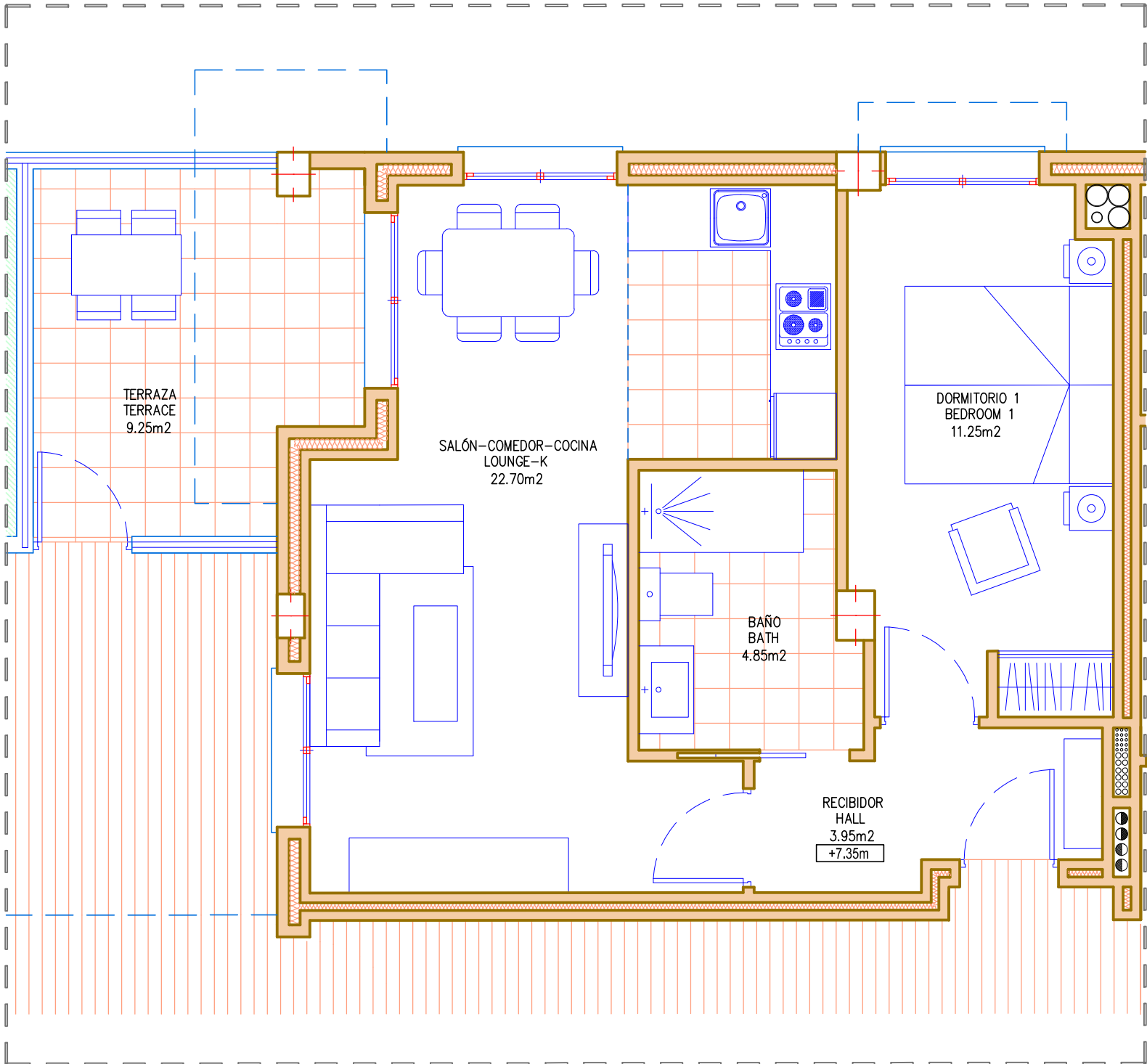
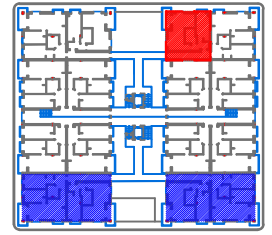
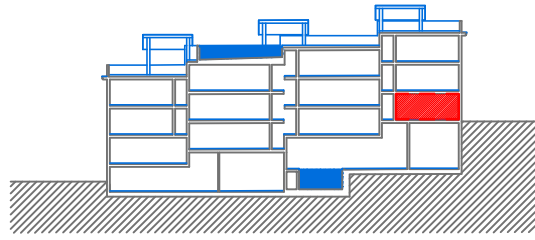
# APATAMENTO Nº 5

SUP. CONSTRUIDA\_ 65.05m<sup>2</sup>  
SUP. UTIL\_ 52.05m<sup>2</sup>



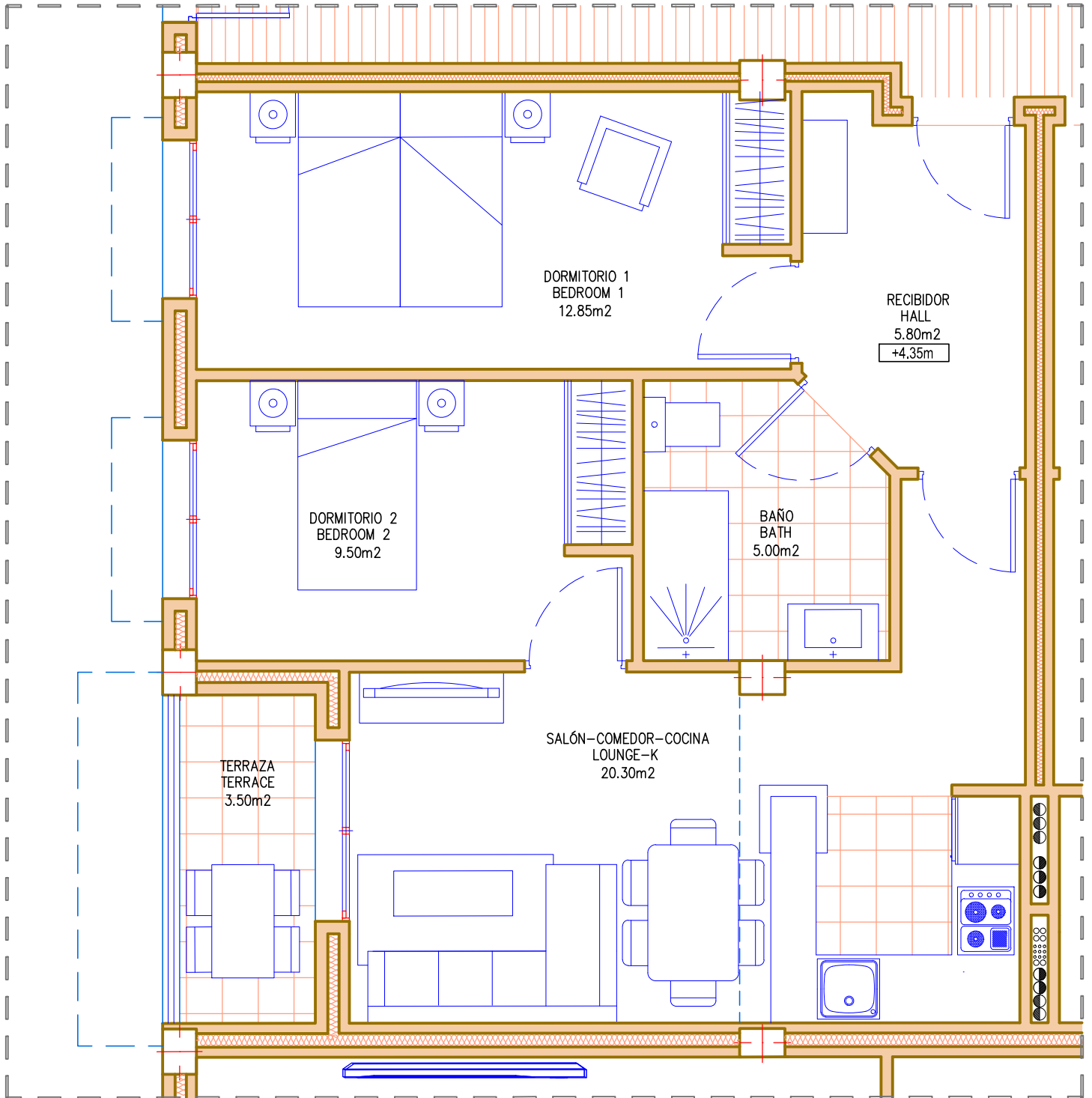
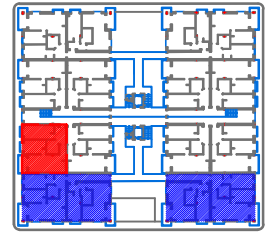
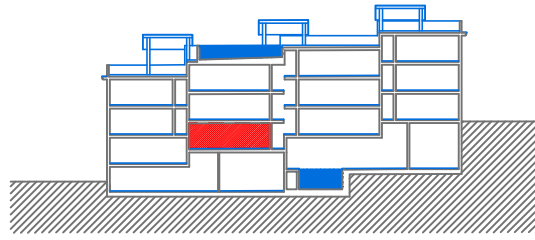
# APATAMENTO N° 6

SUP. CONSTRUIDA\_ 53.70m<sup>2</sup>  
SUP. UTIL\_ 42.75m<sup>2</sup>



# APATAMENTO Nº 7

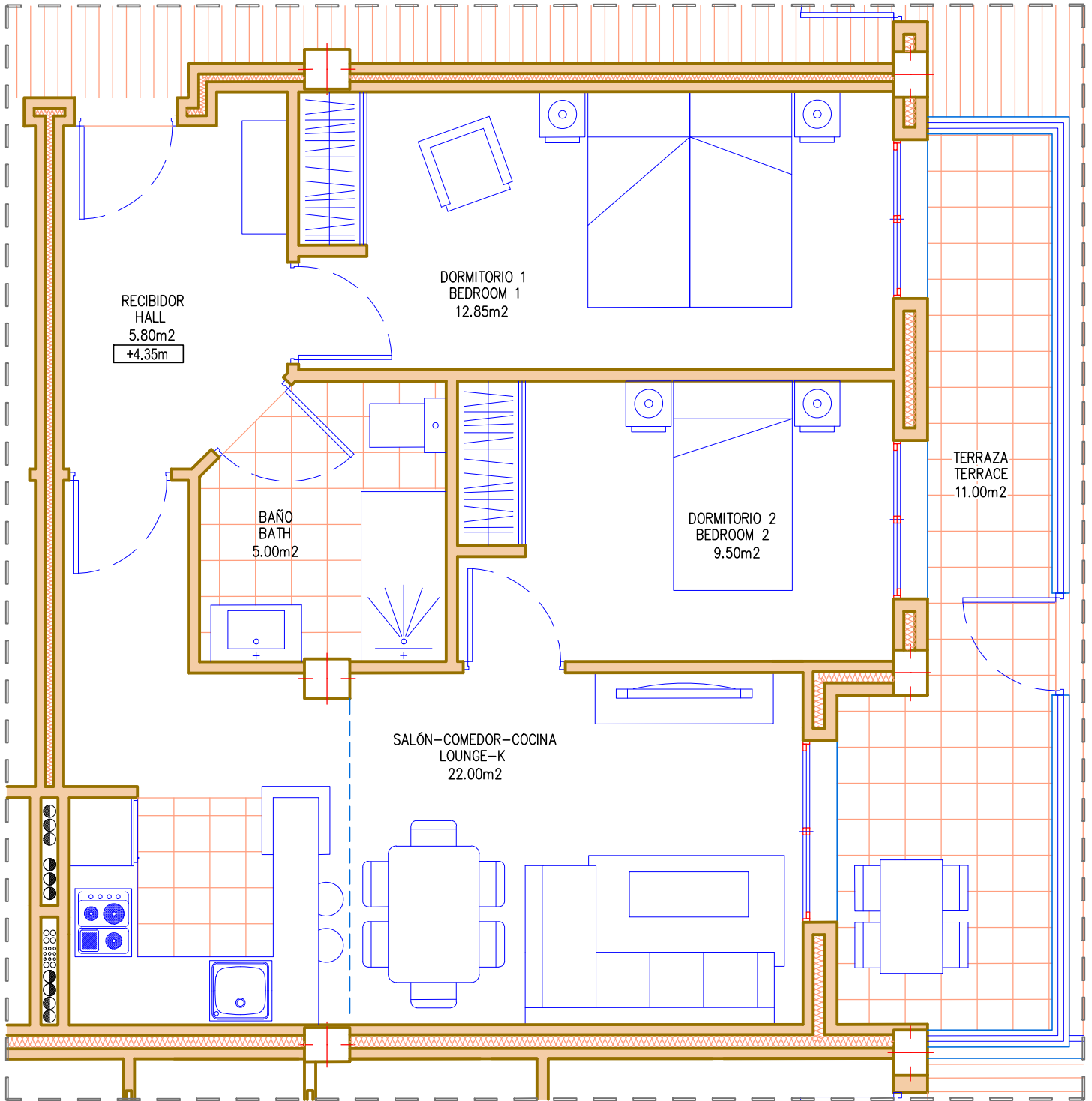
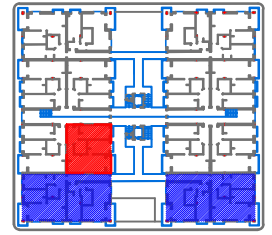
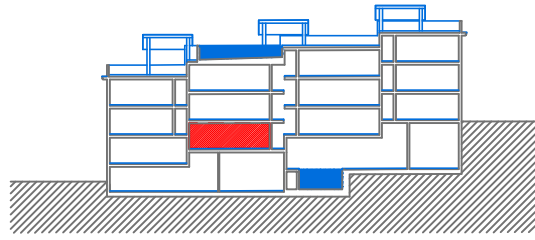
SUP. CONSTRUIDA\_ 66.50m<sup>2</sup>  
SUP. UTIL\_ 53.45m<sup>2</sup>





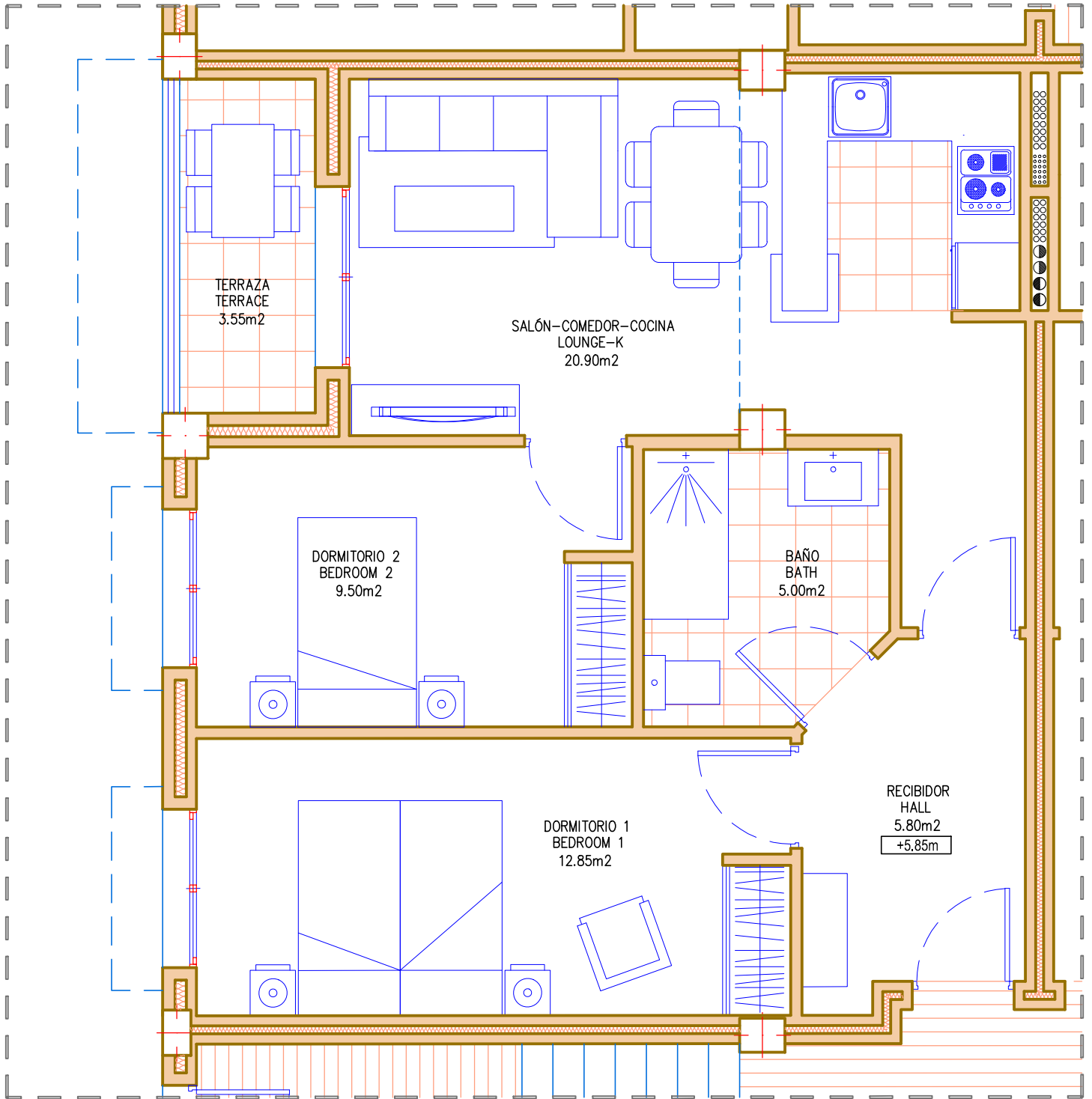
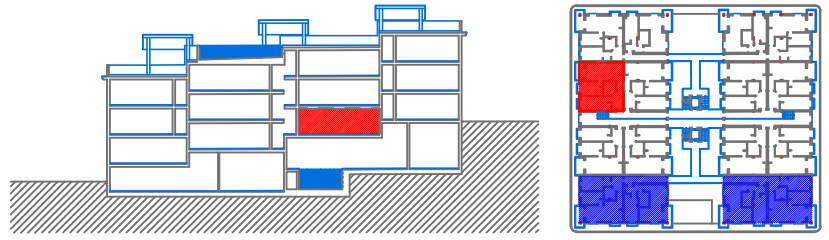
# APATAMENTO Nº 8

SUP. CONSTRUIDA\_ 67.50m<sup>2</sup>  
SUP. UTIL\_ 55.15m<sup>2</sup>



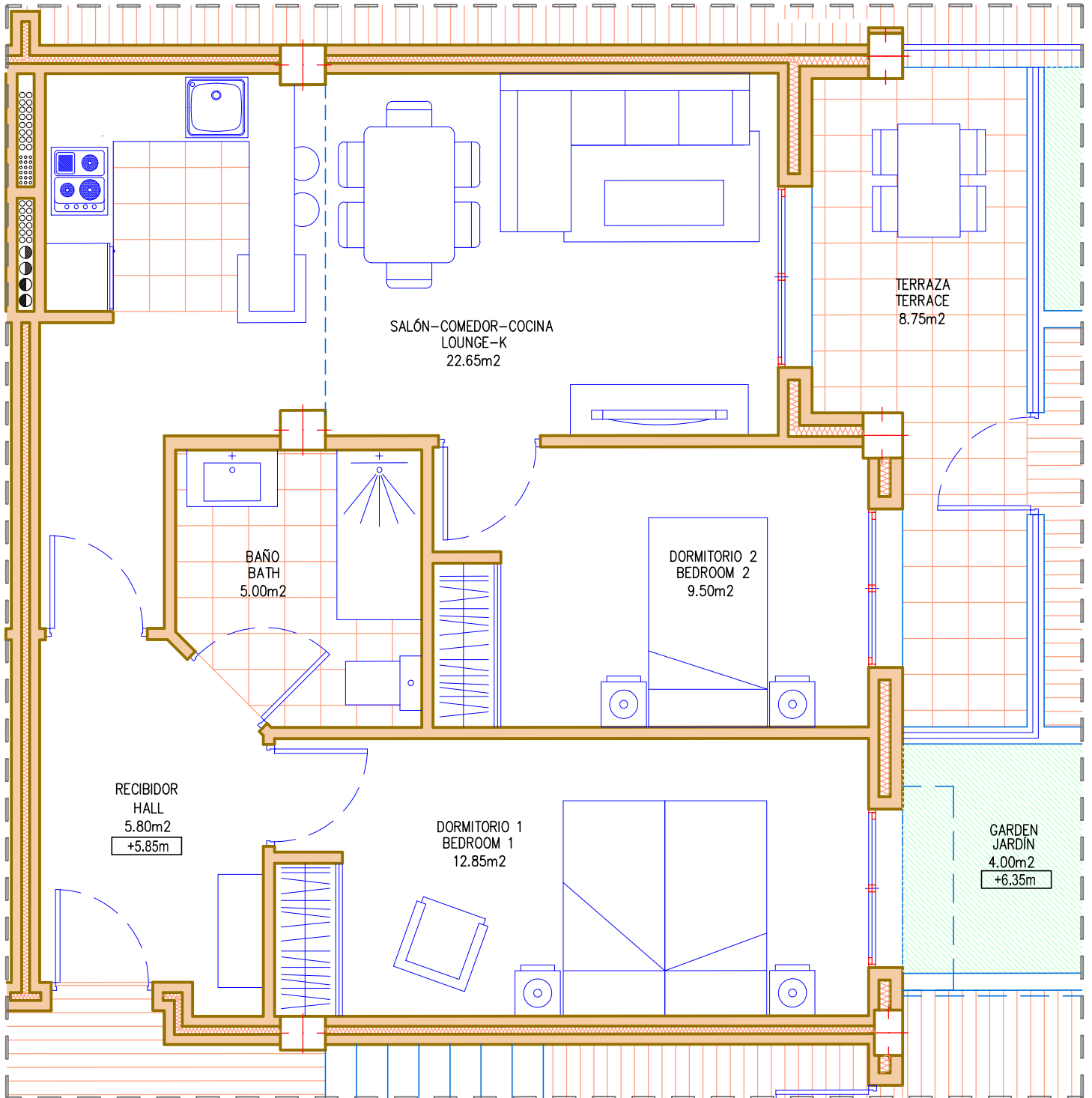
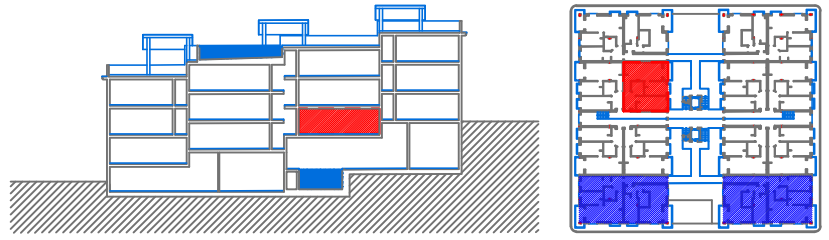
# APATAMENTO Nº 9

SUP. CONSTRUIDA\_ 67.05m<sup>2</sup>  
SUP. UTIL\_ 54.05m<sup>2</sup>



# APATAMENTO N° 10

SUP. CONSTRUIDA\_ 68.00m<sup>2</sup>  
SUP. UTIL\_ 55.85m<sup>2</sup>

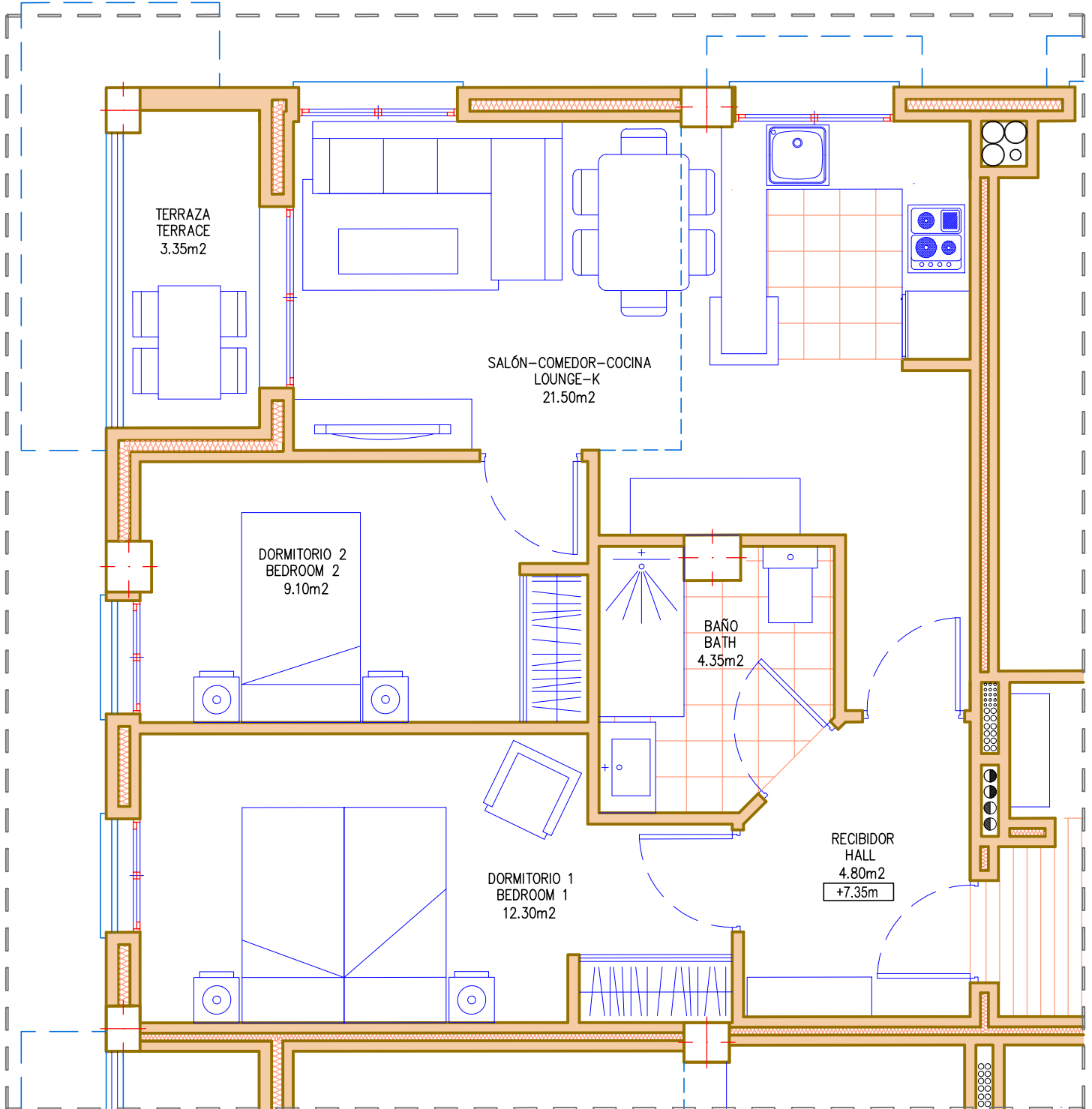
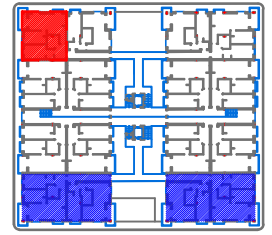
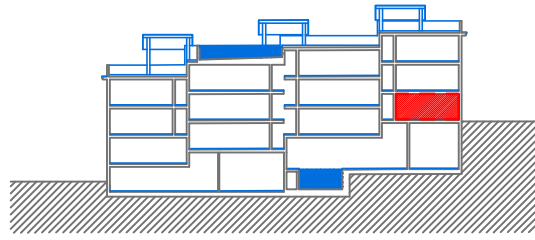


# APATAMENTO Nº 11

SUP. CONSTRUIDA\_ 65.05m<sup>2</sup>  
SUP. UTIL\_ 52.05m<sup>2</sup>



Escala\_ 1/50

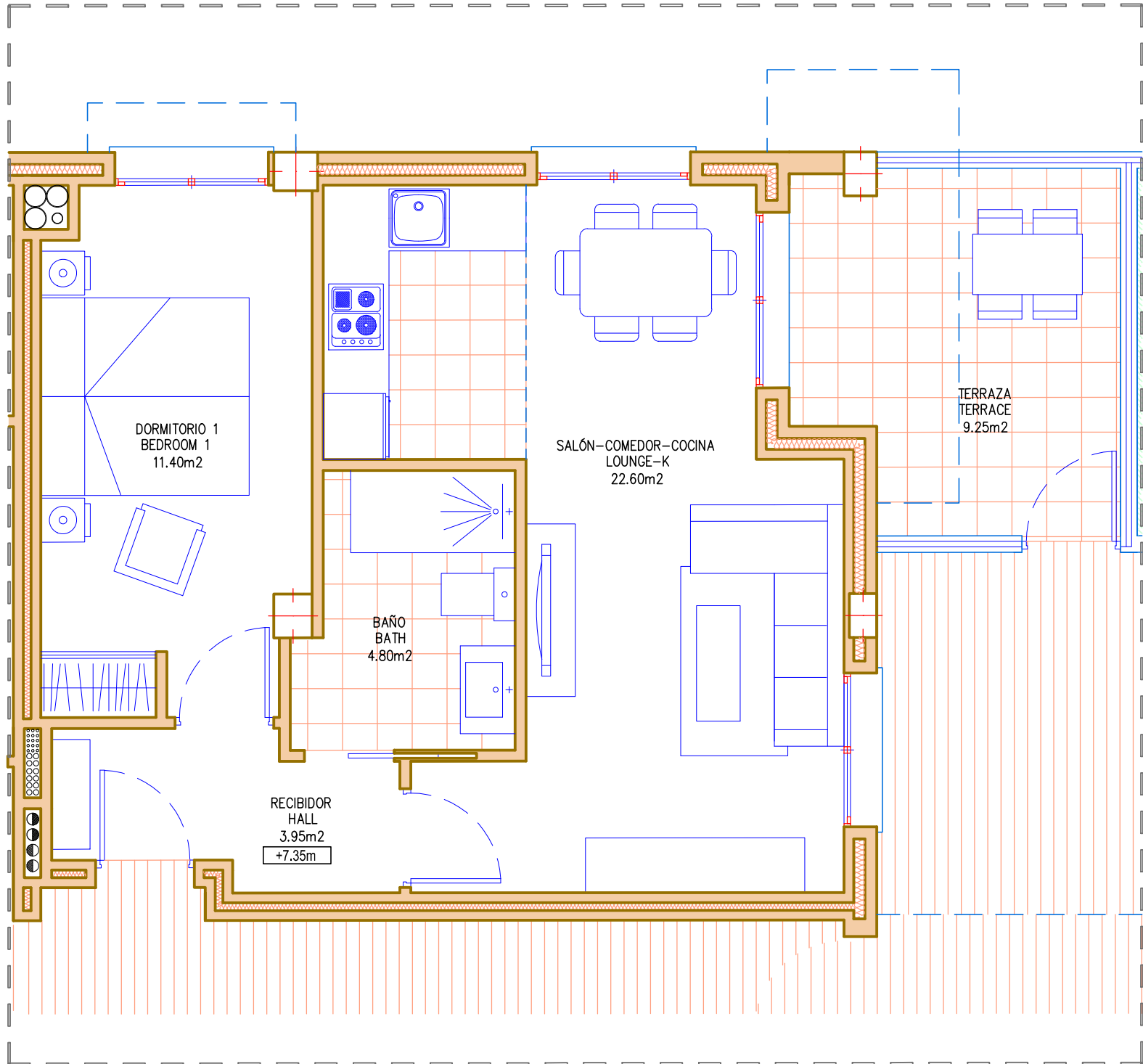
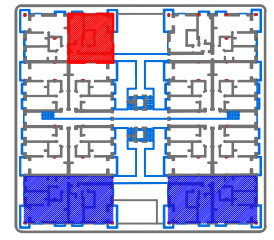
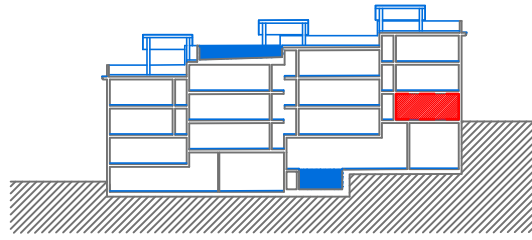


# APATAMENTO N° 12

SUP. CONSTRUIDA\_ 53.70m<sup>2</sup>  
SUP. UTIL\_ 42.75m<sup>2</sup>

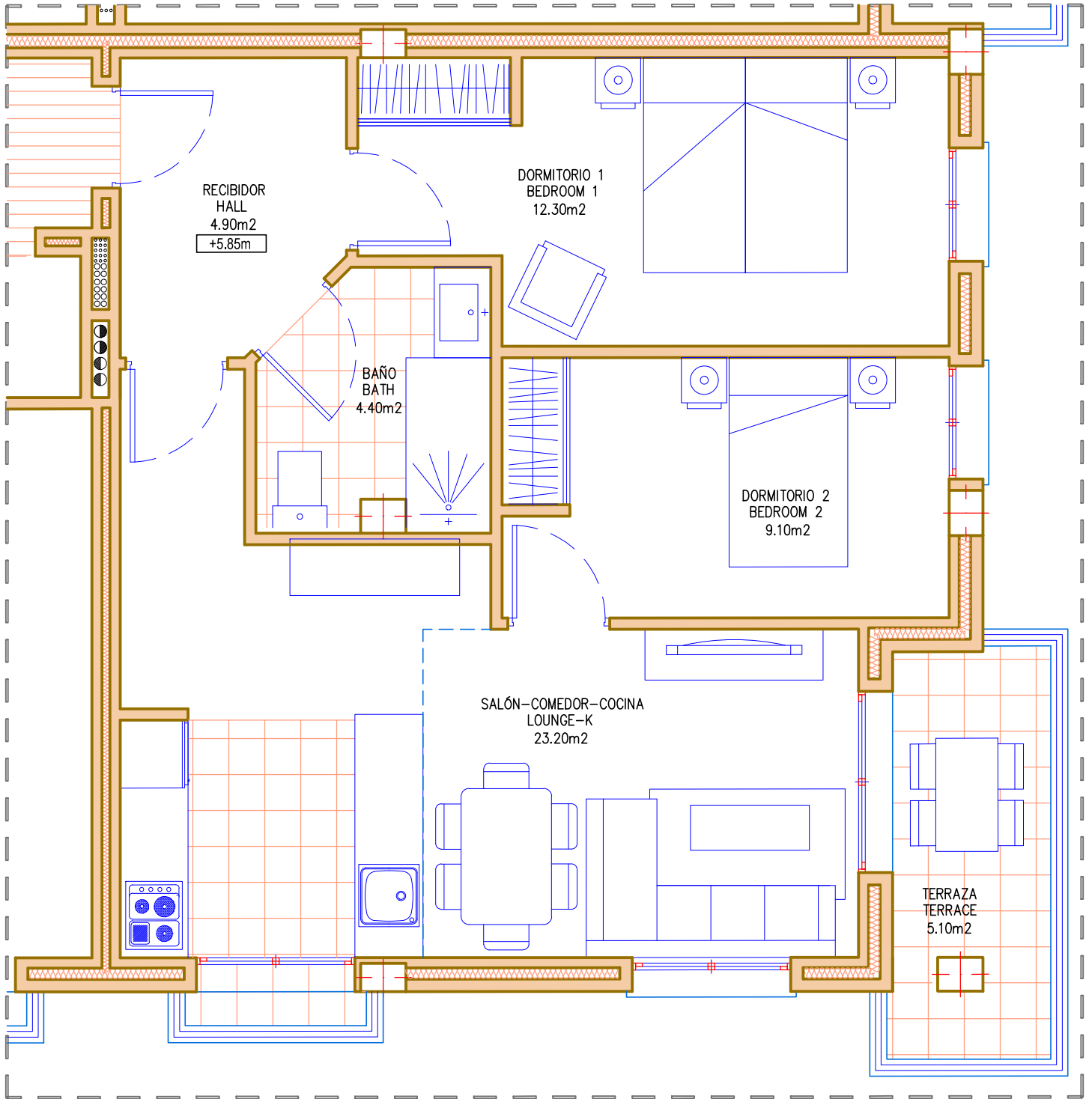
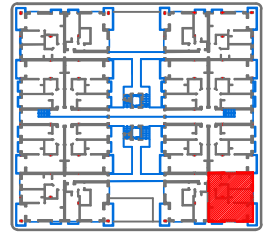
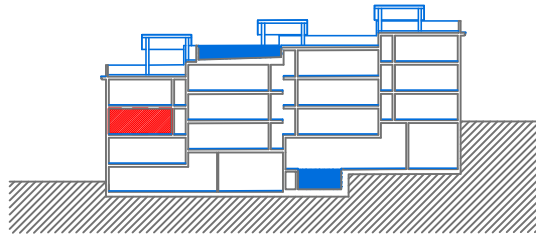


Escala\_ 1/50



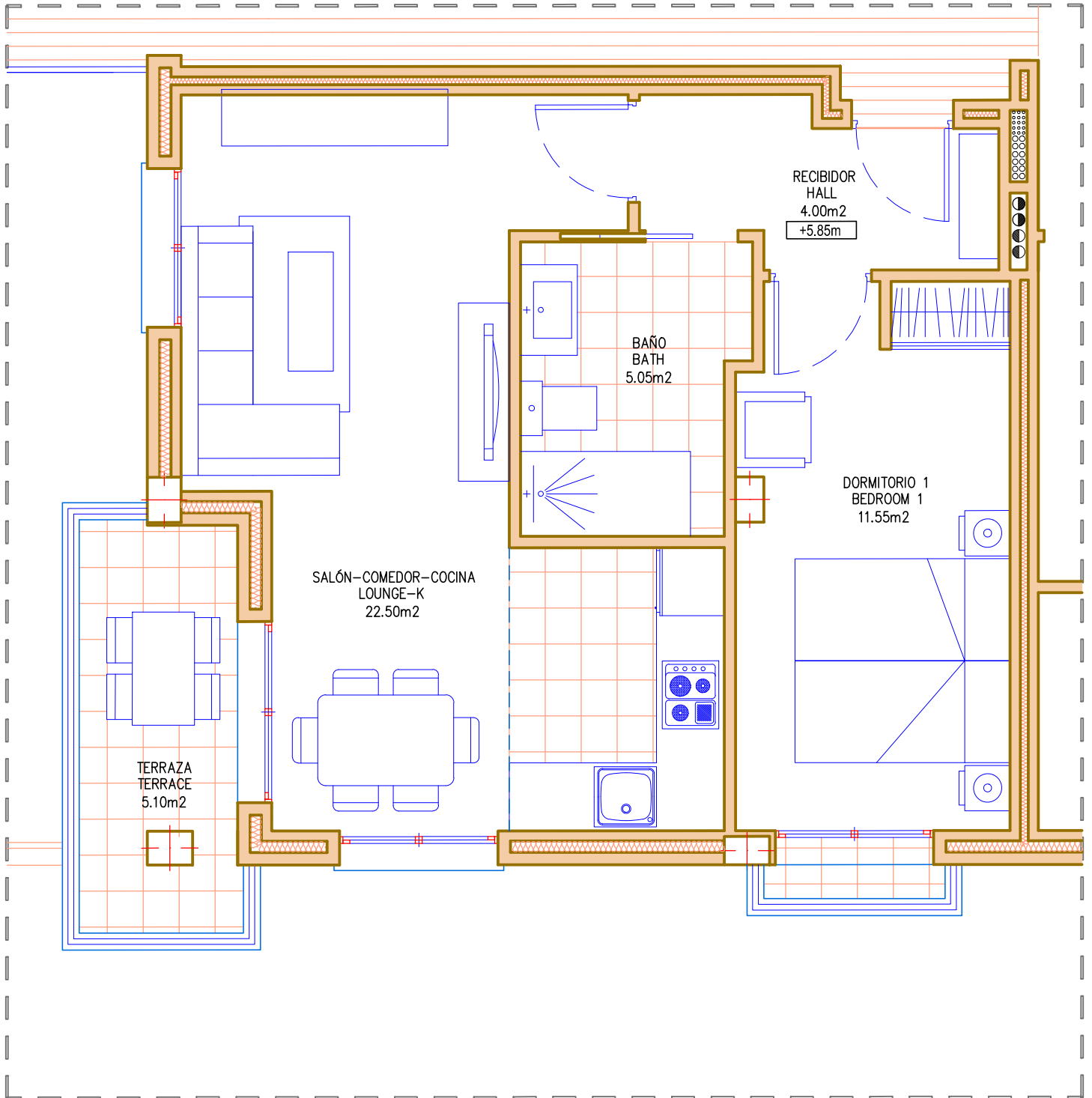
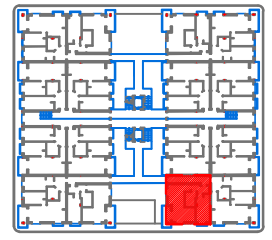
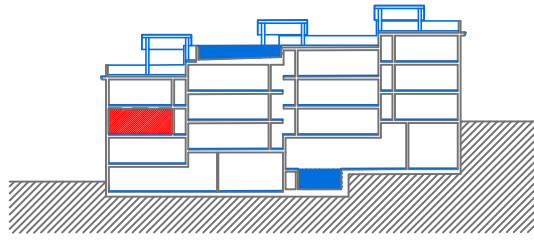
# APATAMENTO N° 13

SUP. CONSTRUIDA\_ 65.30m<sup>2</sup>  
SUP. UTIL\_ 53.80m<sup>2</sup>



# APATAMENTO N° 14

SUP. CONSTRUIDA\_ 54.05m<sup>2</sup>  
SUP. UTIL\_ 43.10m<sup>2</sup>

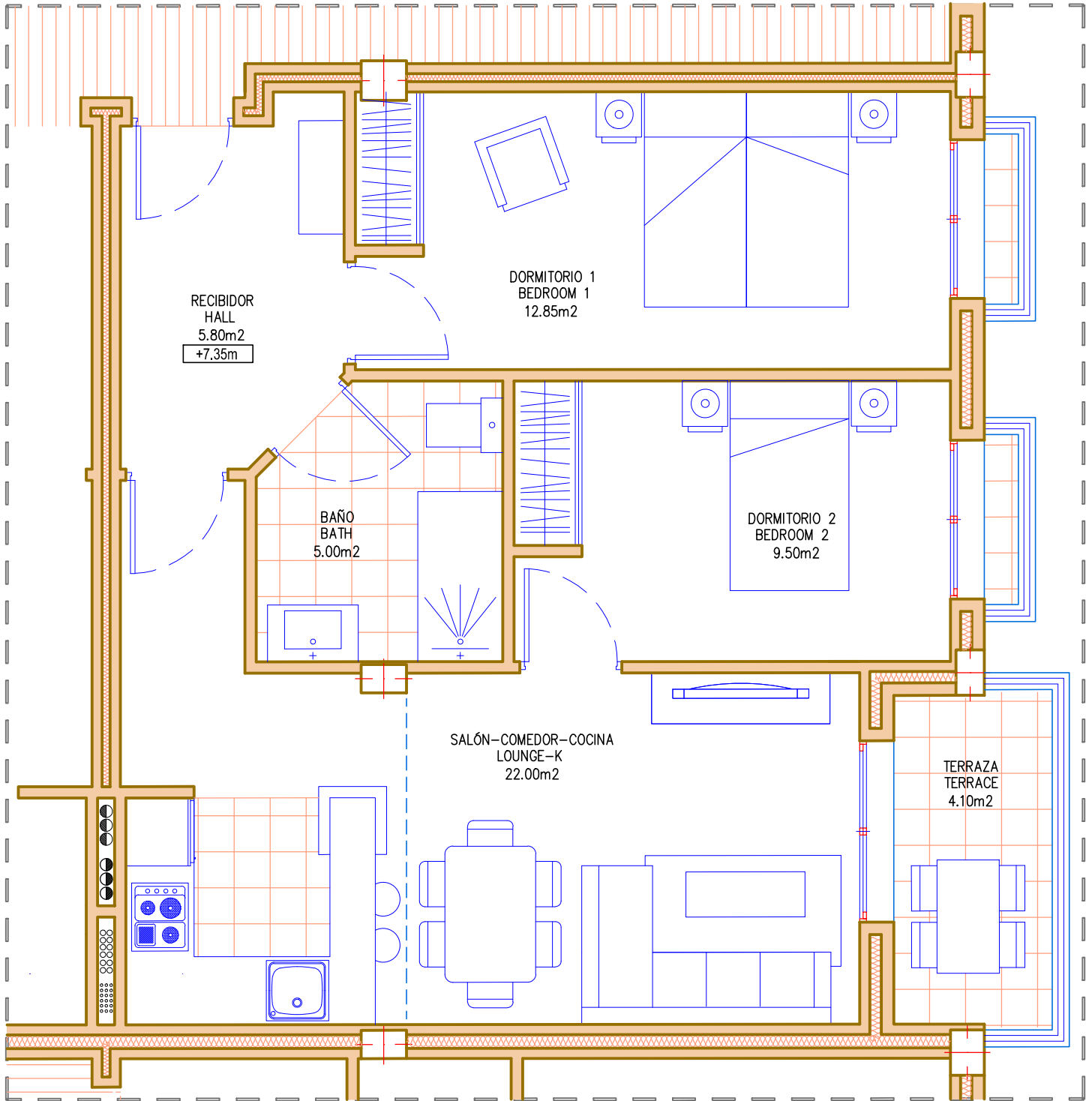
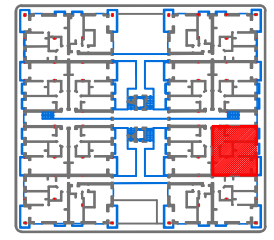
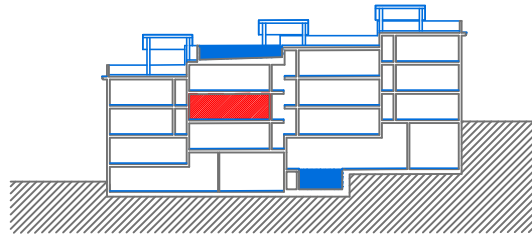


# APATAMENTO Nº 15

SUP. CONSTRUIDA\_ 66.30m<sup>2</sup>  
SUP. UTIL\_ 55.30m<sup>2</sup>



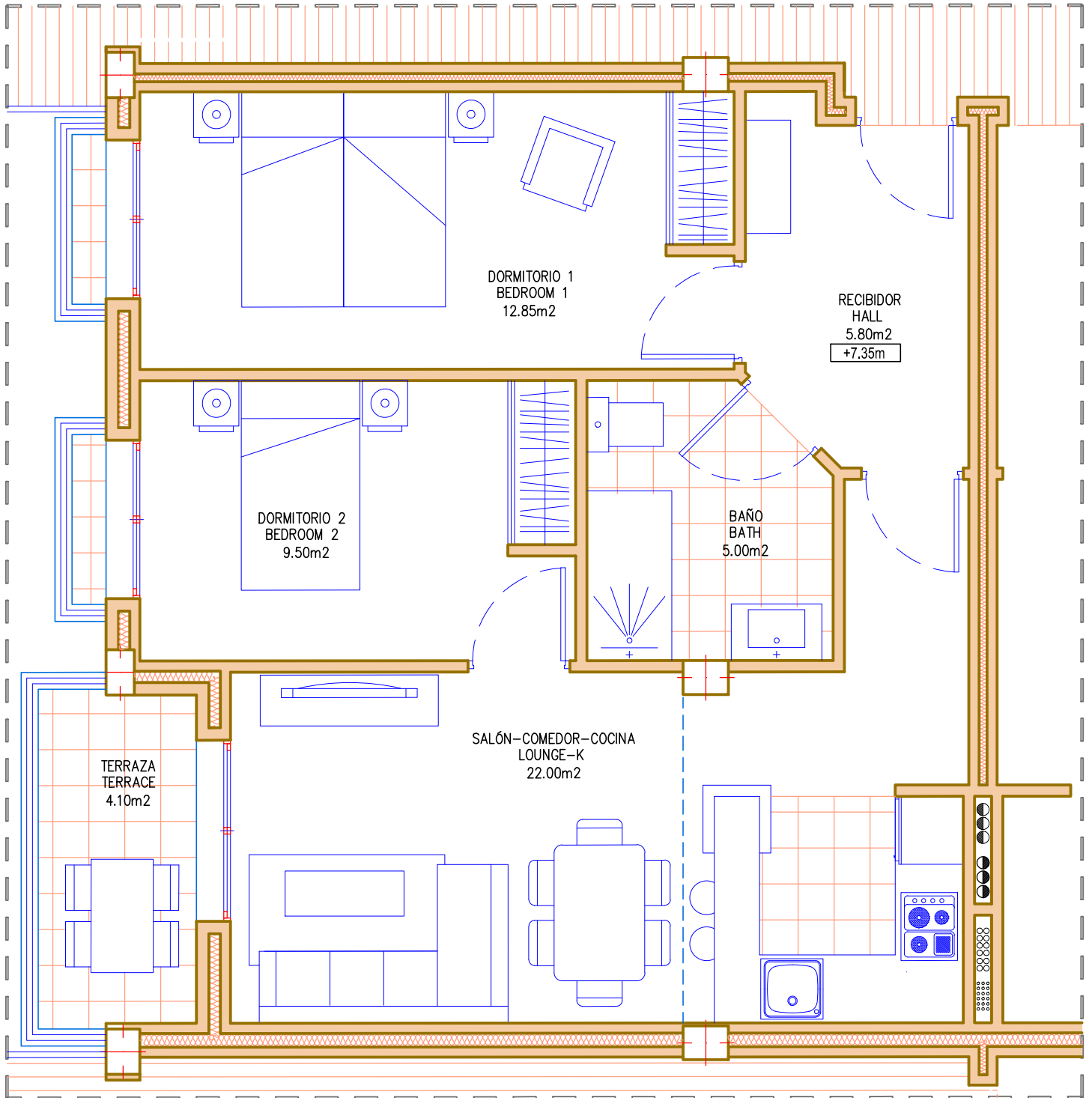
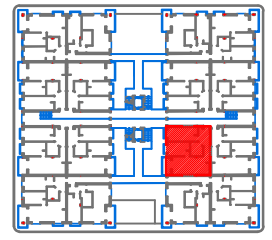
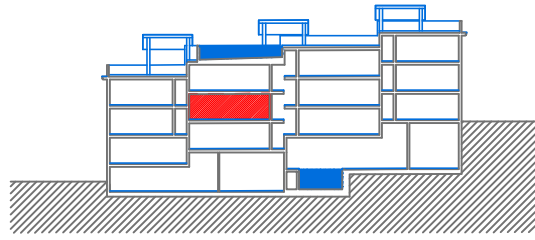
Escala\_ 1/50





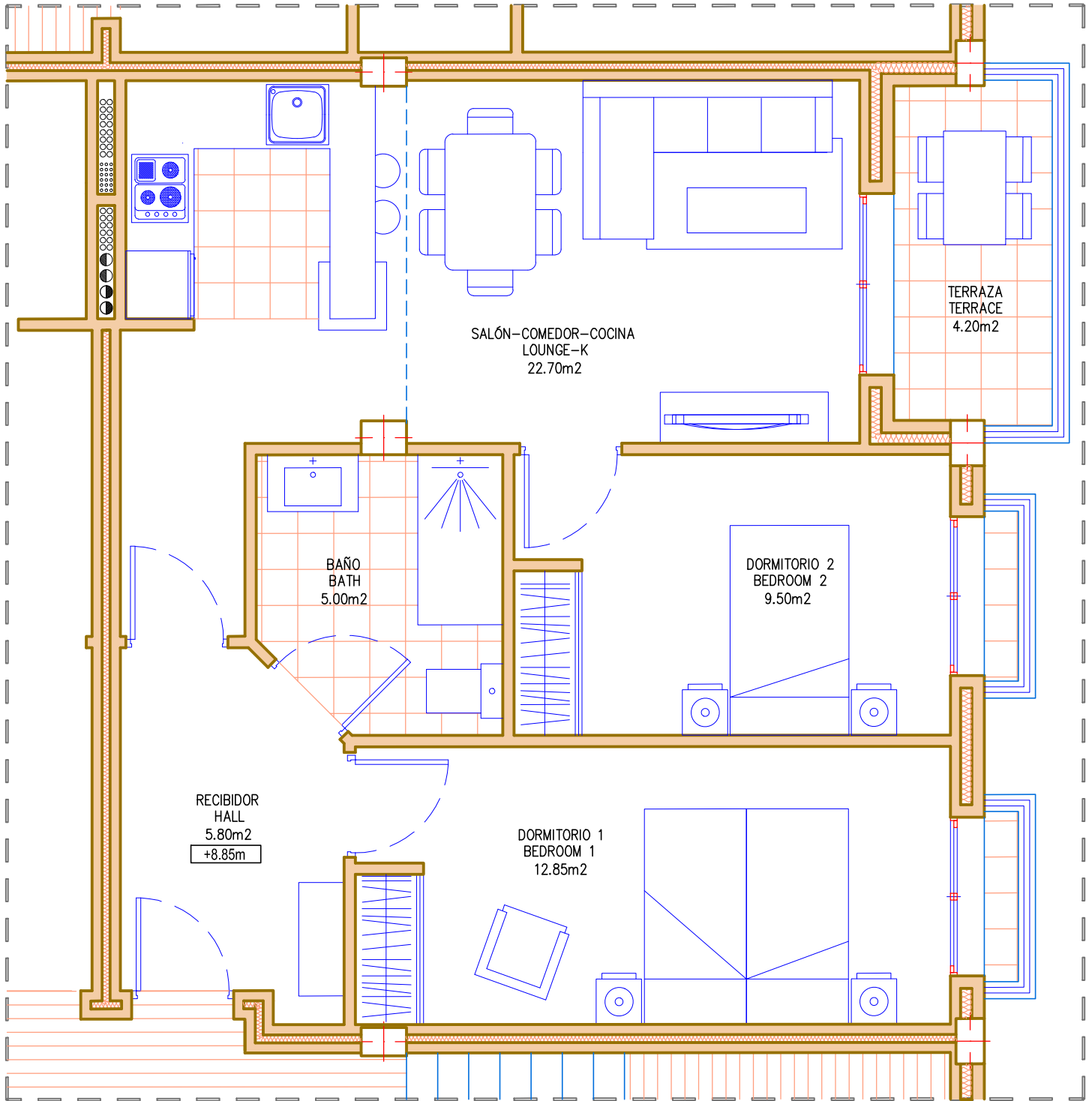
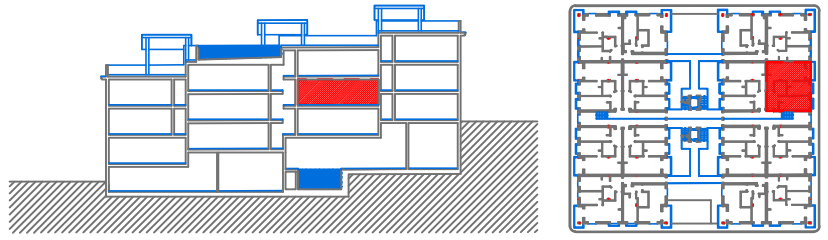
# APATAMENTO Nº 16

SUP. CONSTRUIDA\_ 66.30m<sup>2</sup>  
SUP. UTIL\_ 55.30m<sup>2</sup>



# APATAMENTO N° 17

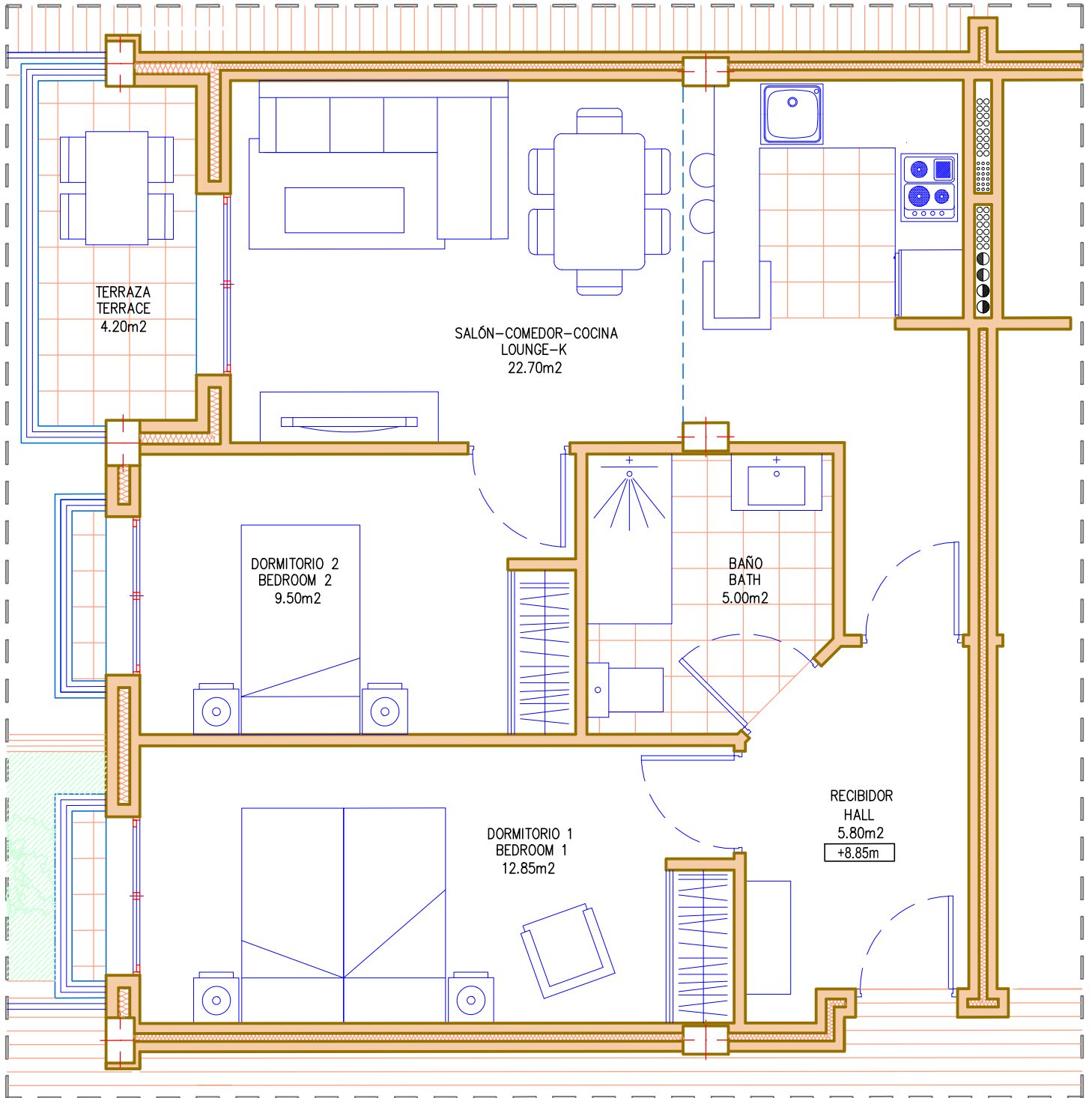
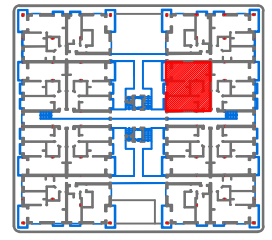
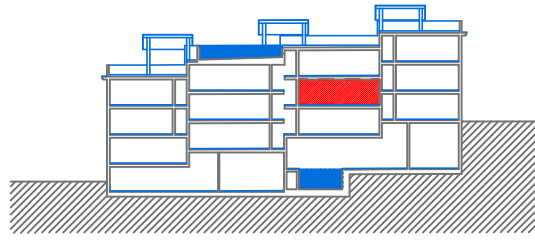
SUP. CONSTRUIDA\_ 67.05m<sup>2</sup>  
SUP. UTIL\_ 55.85m<sup>2</sup>



# APATAMENTO N° 18

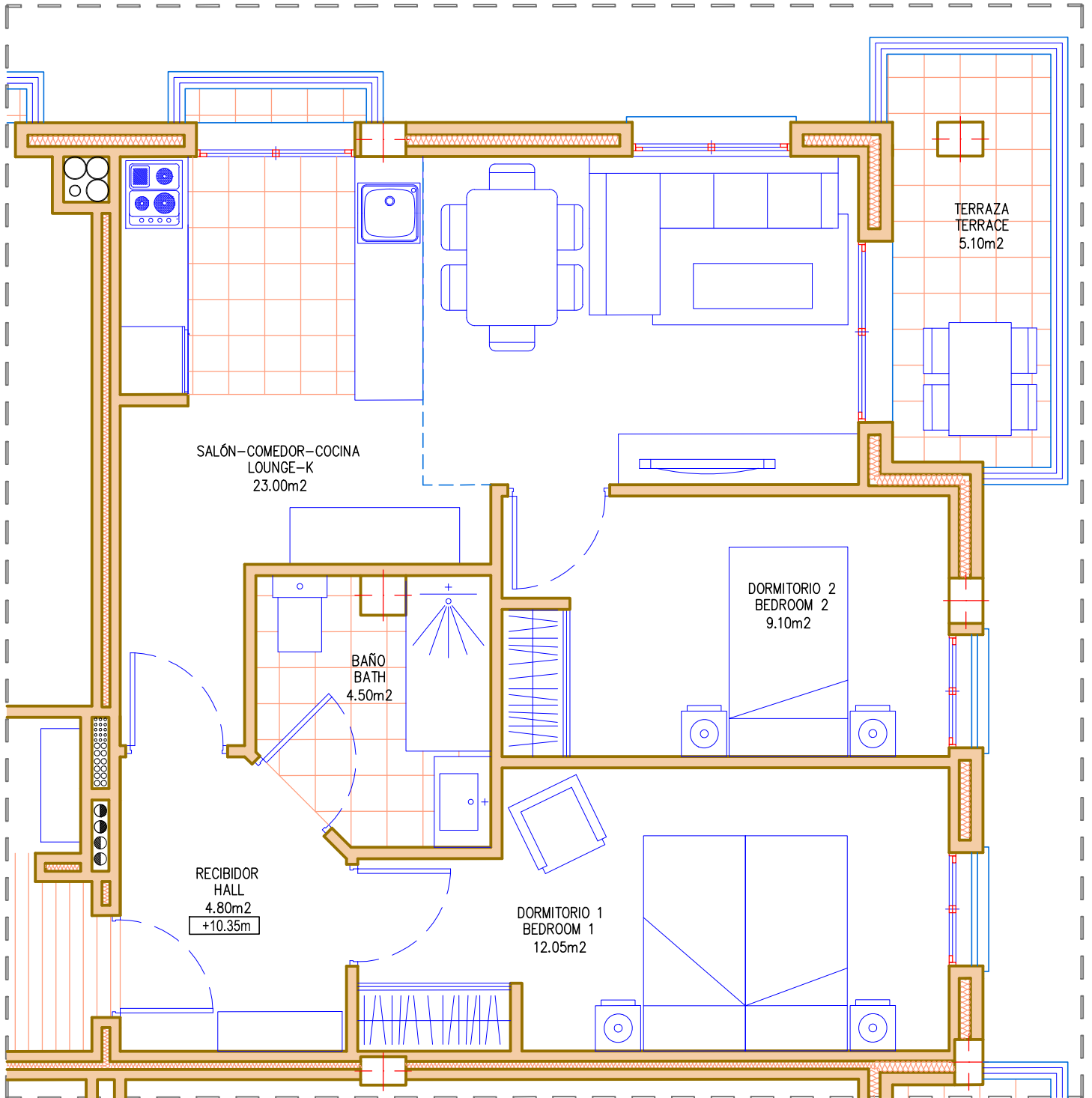
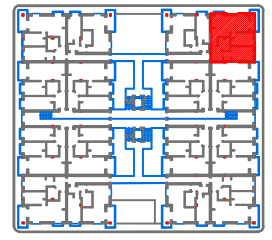
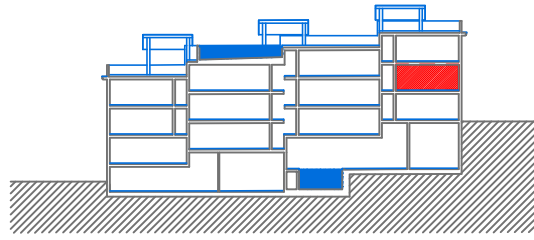
SUP. CONSTRUIDA\_ 68.00m<sup>2</sup>  
SUP. UTIL\_ 55.85m<sup>2</sup>

Escala\_ 1/50



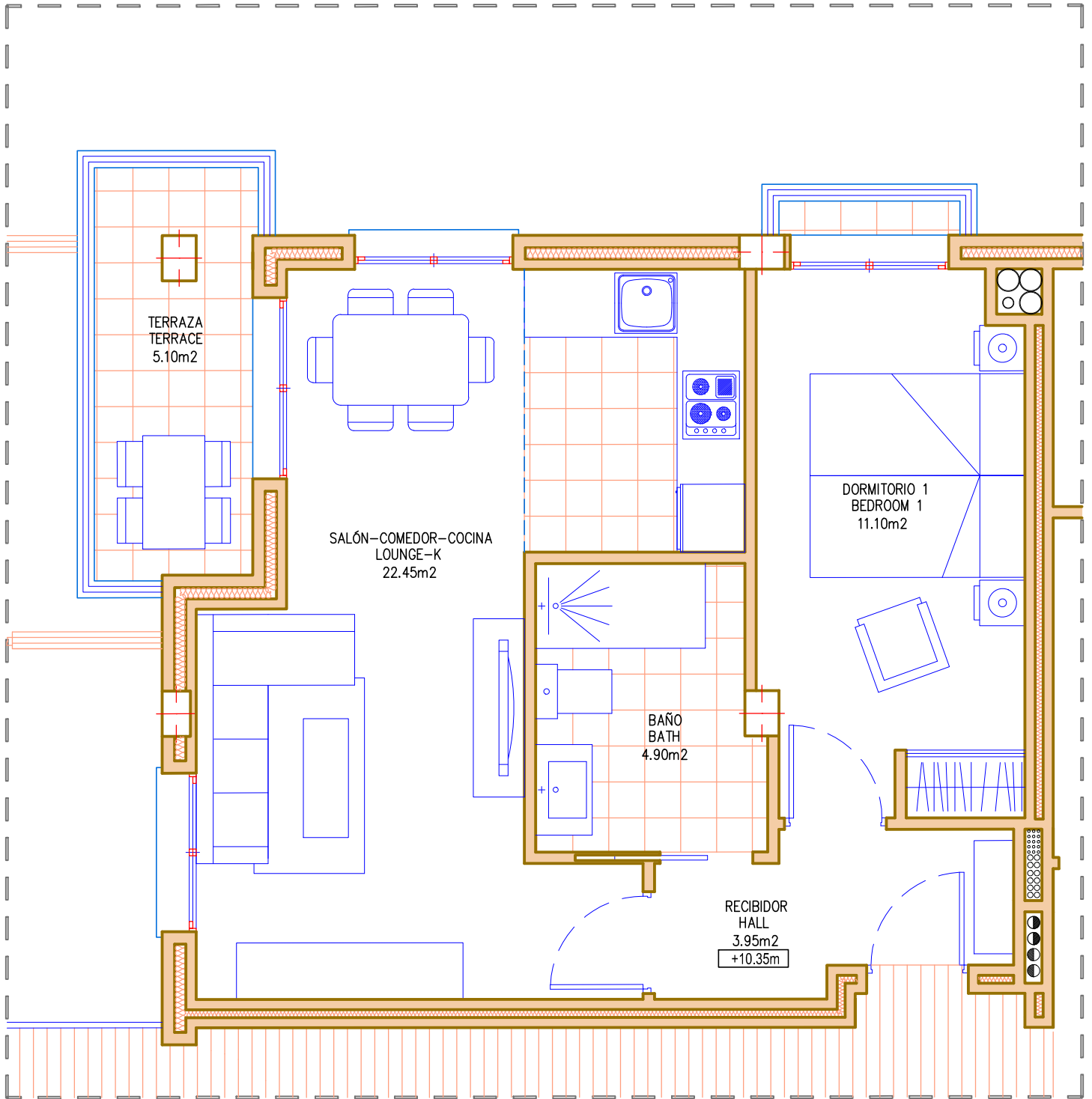
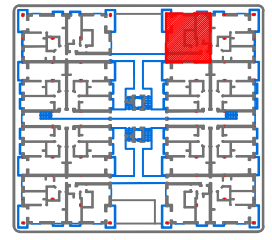
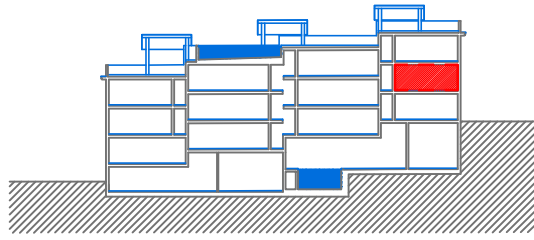
# APATAMENTO Nº 19

SUP. CONSTRUIDA\_ 64.80m<sup>2</sup>  
SUP. UTIL\_ 53.45m<sup>2</sup>



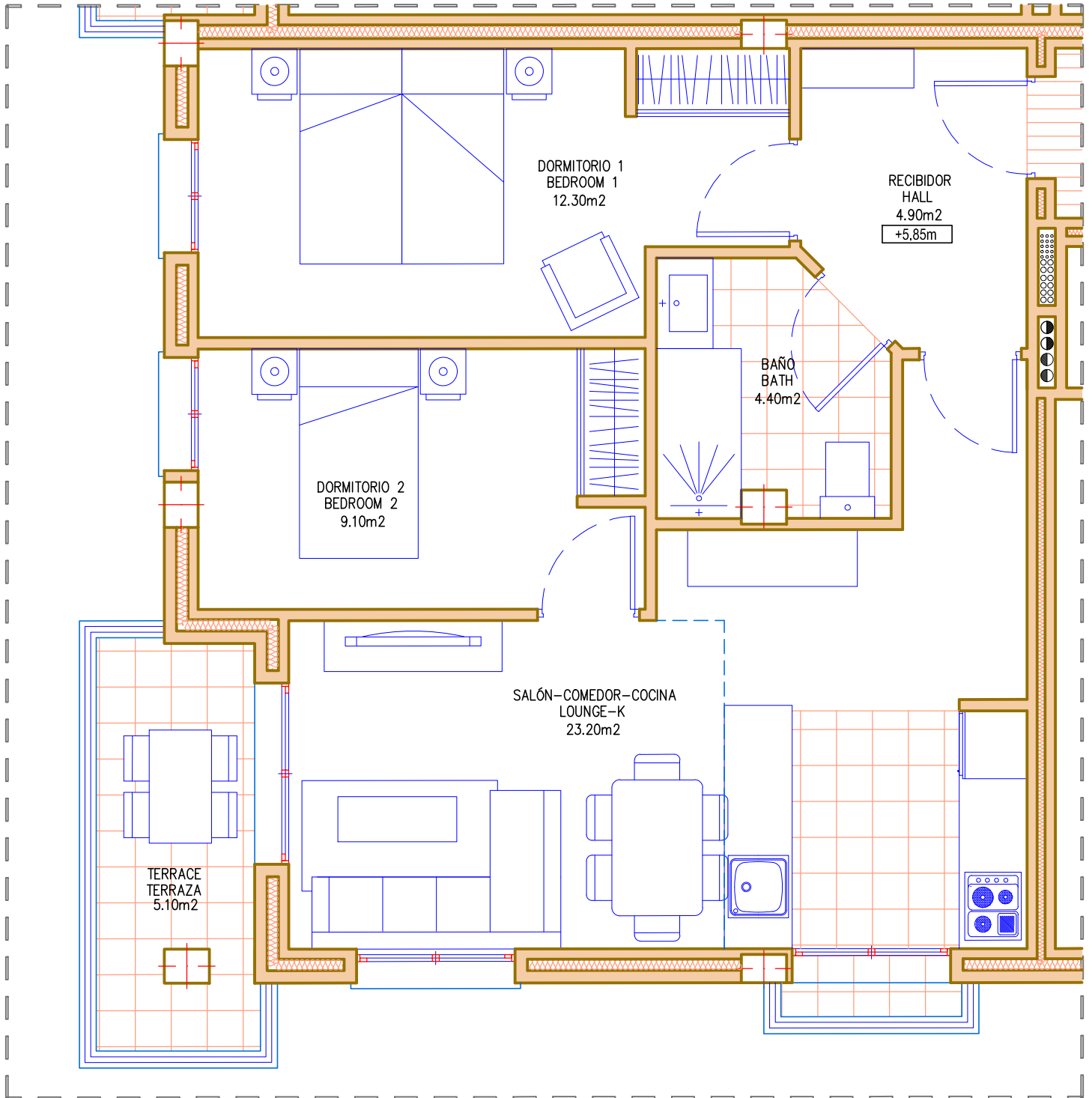
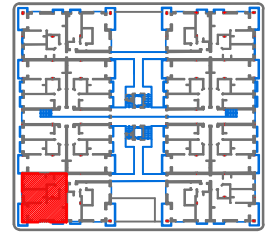
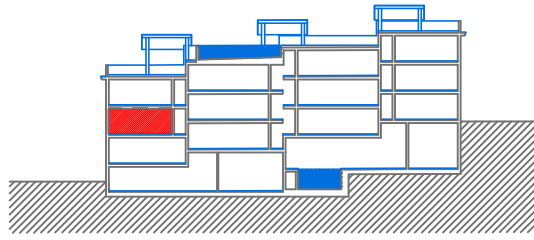
# APATAMENTO N° 20

SUP. CONSTRUIDA\_ 53.70m<sup>2</sup>  
SUP. UTIL\_ 42.40m<sup>2</sup>



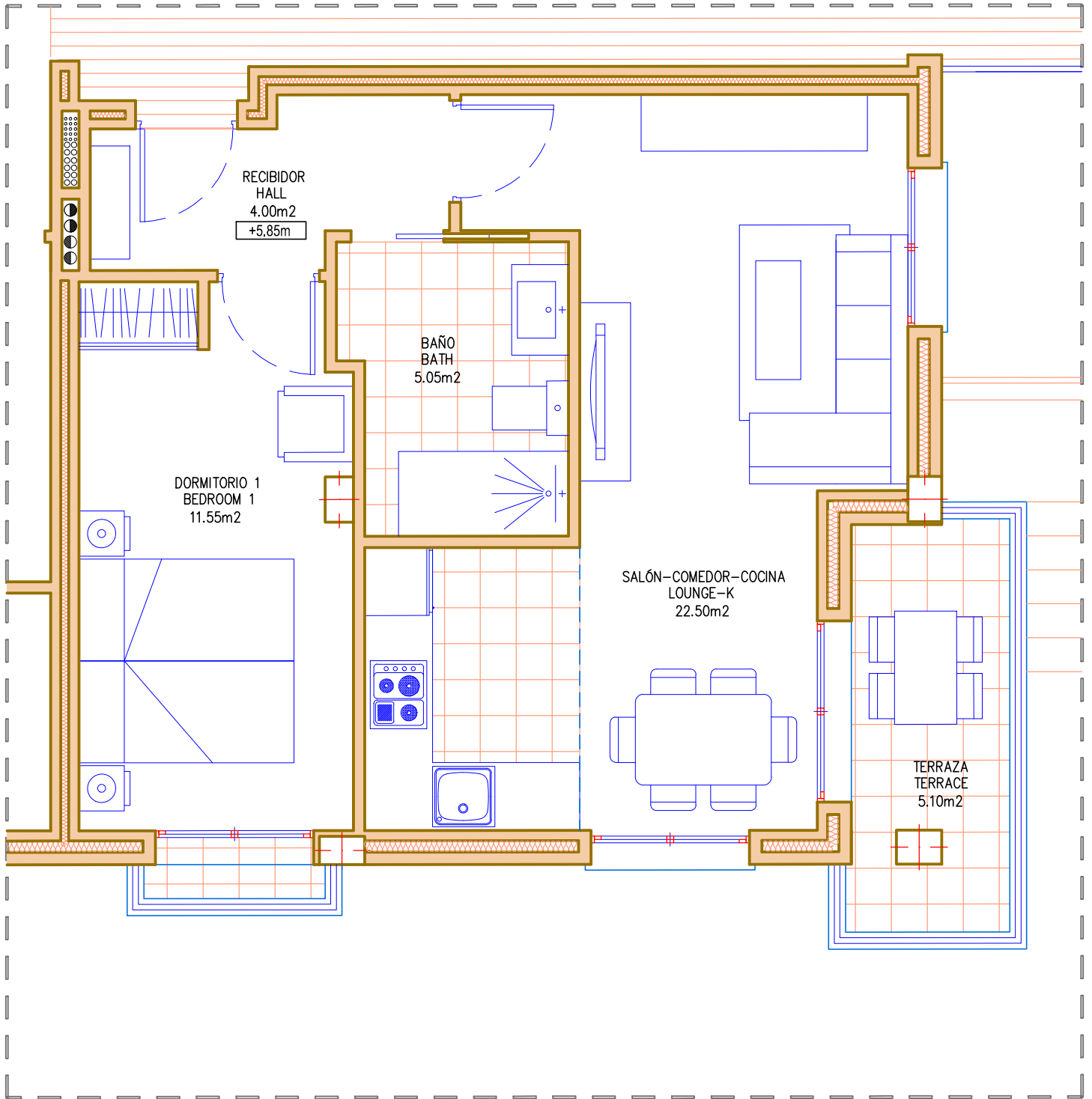
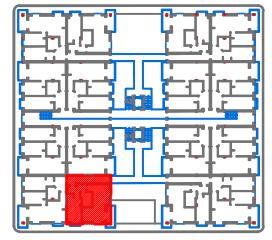
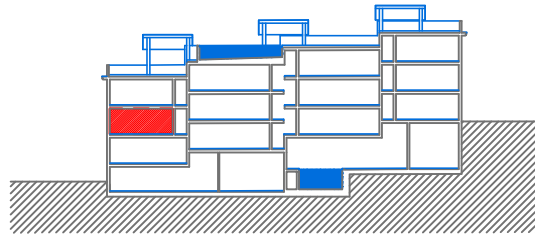
# APATAMENTO N° 21

SUP. CONSTRUIDA\_ 65.30m<sup>2</sup>  
SUP. UTIL\_ 53.90m<sup>2</sup>



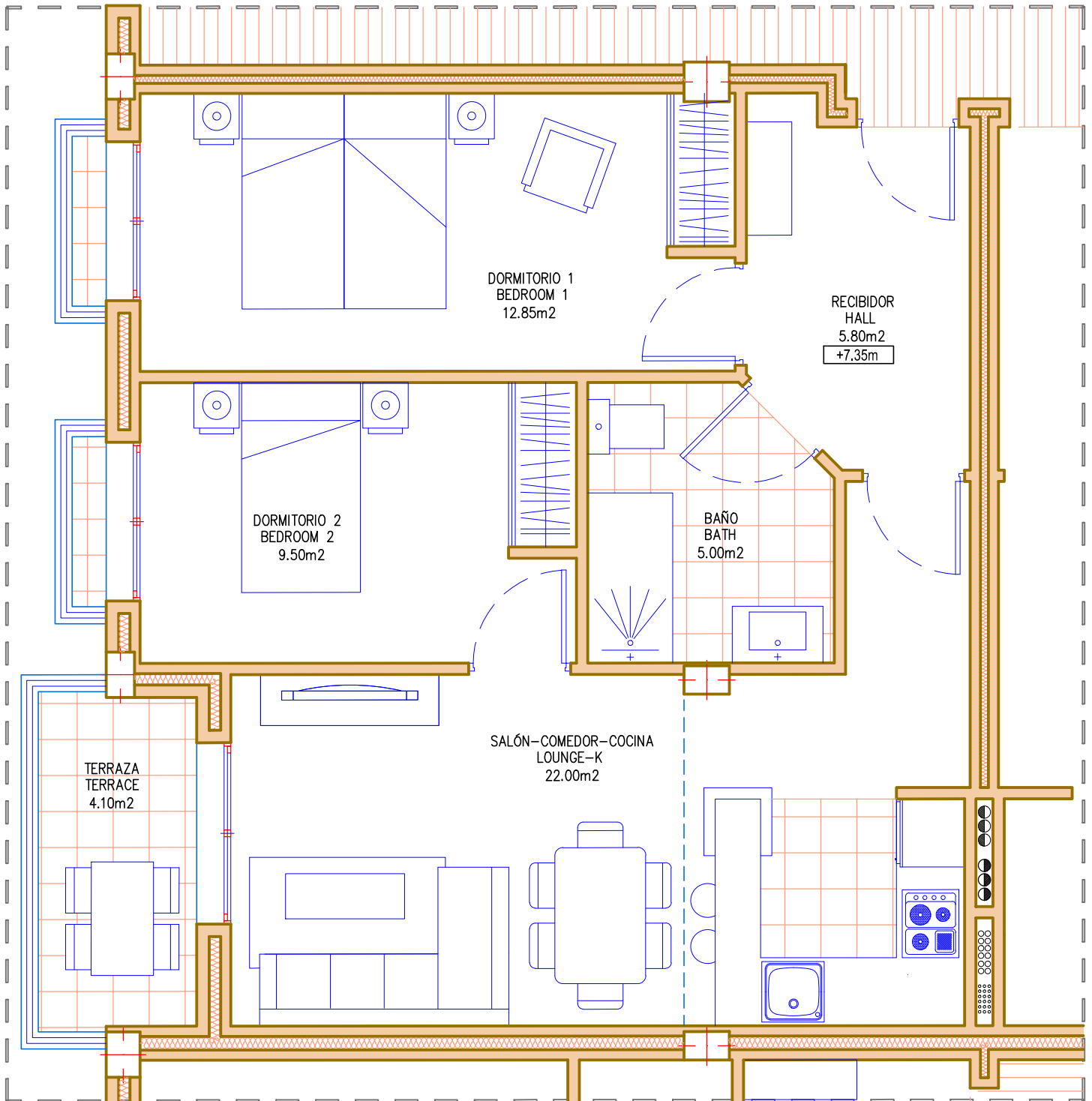
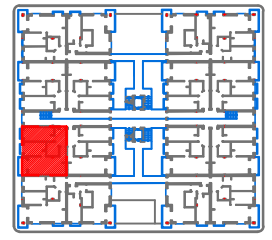
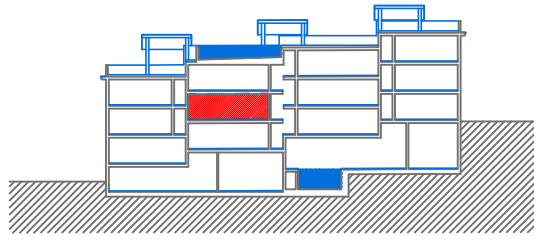
# APATAMENTO N° 22

SUP. CONSTRUIDA\_ 54.40m<sup>2</sup>  
SUP. UTIL\_ 43.10m<sup>2</sup>



# APATAMENTO N° 23

SUP. CONSTRUIDA\_ 66.50m<sup>2</sup>  
SUP. UTIL\_ 55.15m<sup>2</sup>



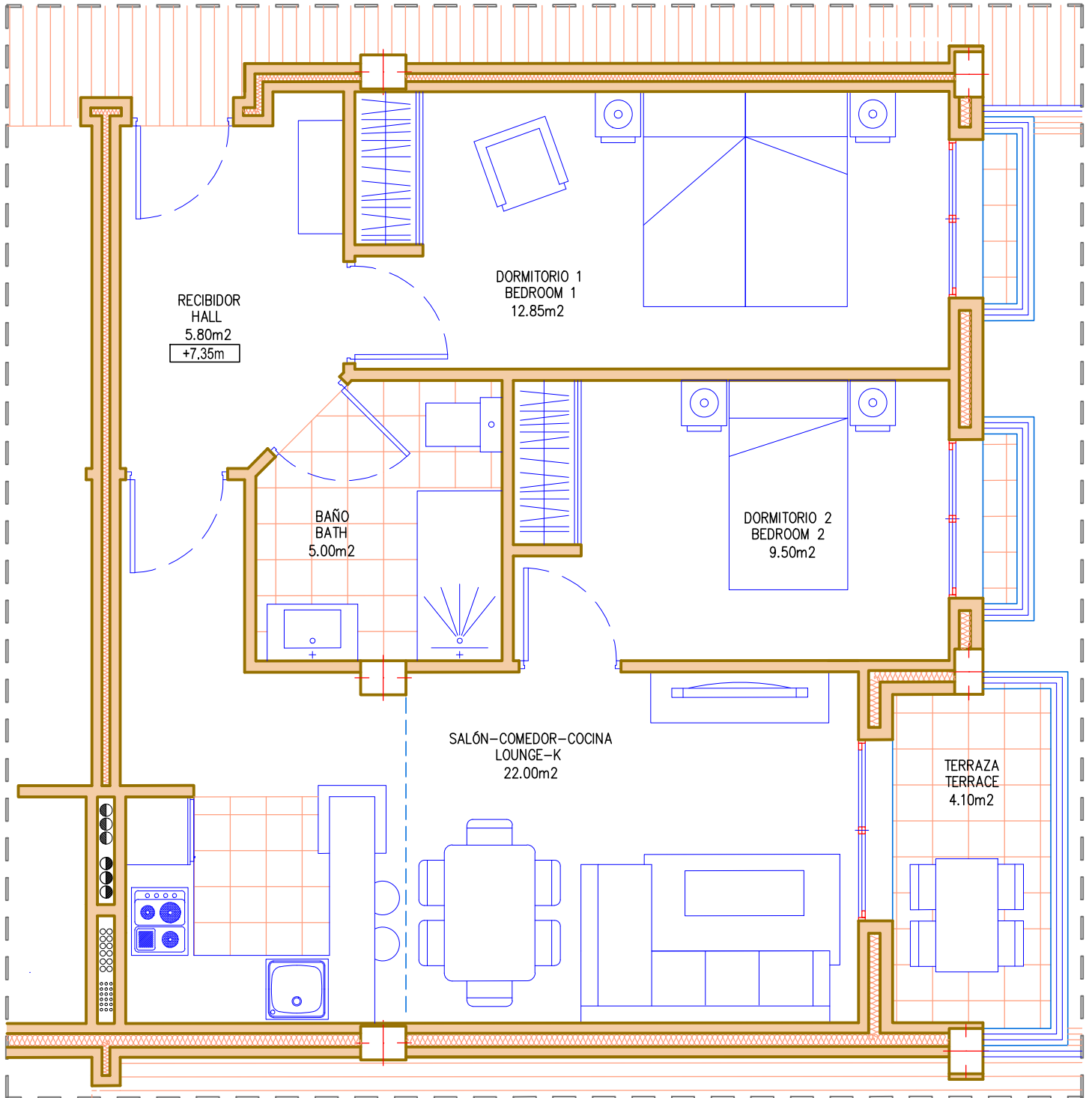
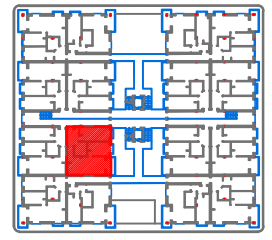
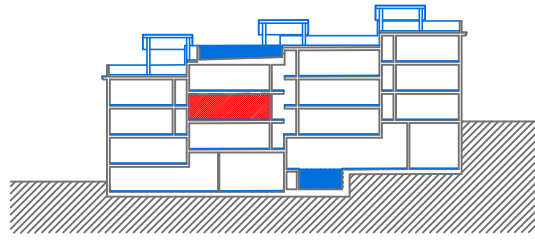


# APATAMENTO N° 24

SUP. CONSTRUIDA\_ 67.60m<sup>2</sup>  
SUP. UTIL\_ 55.15m<sup>2</sup>

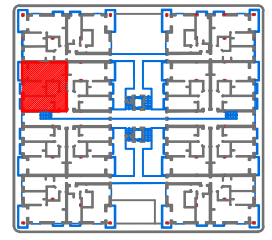
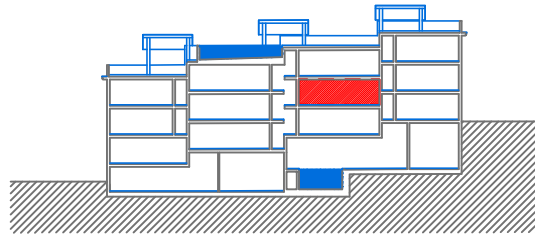


Escala\_ 1/50

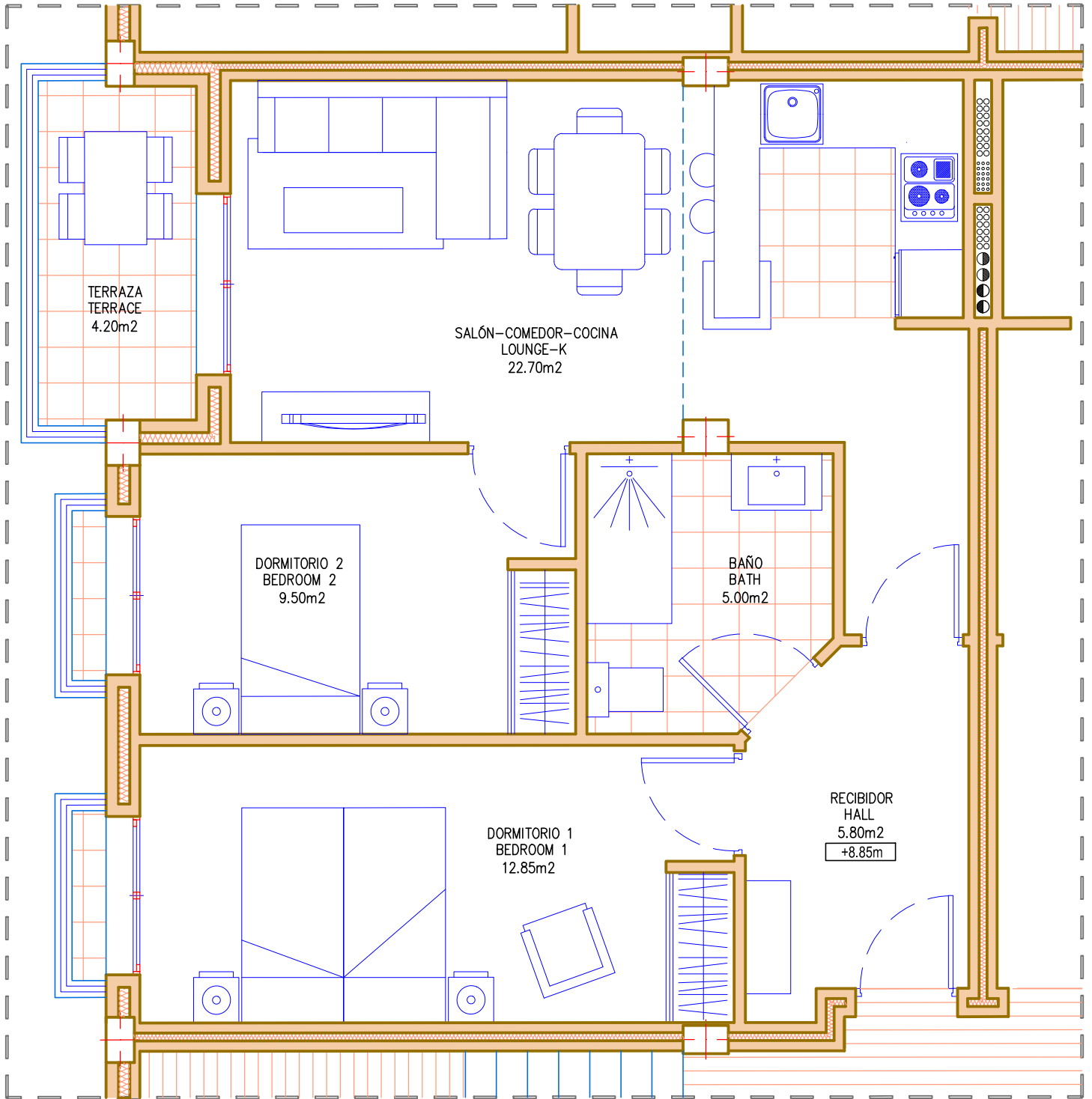


# APATAMENTO N° 25

SUP. CONSTRUIDA\_ 67.05m<sup>2</sup>  
SUP. UTIL\_ 55.85m<sup>2</sup>

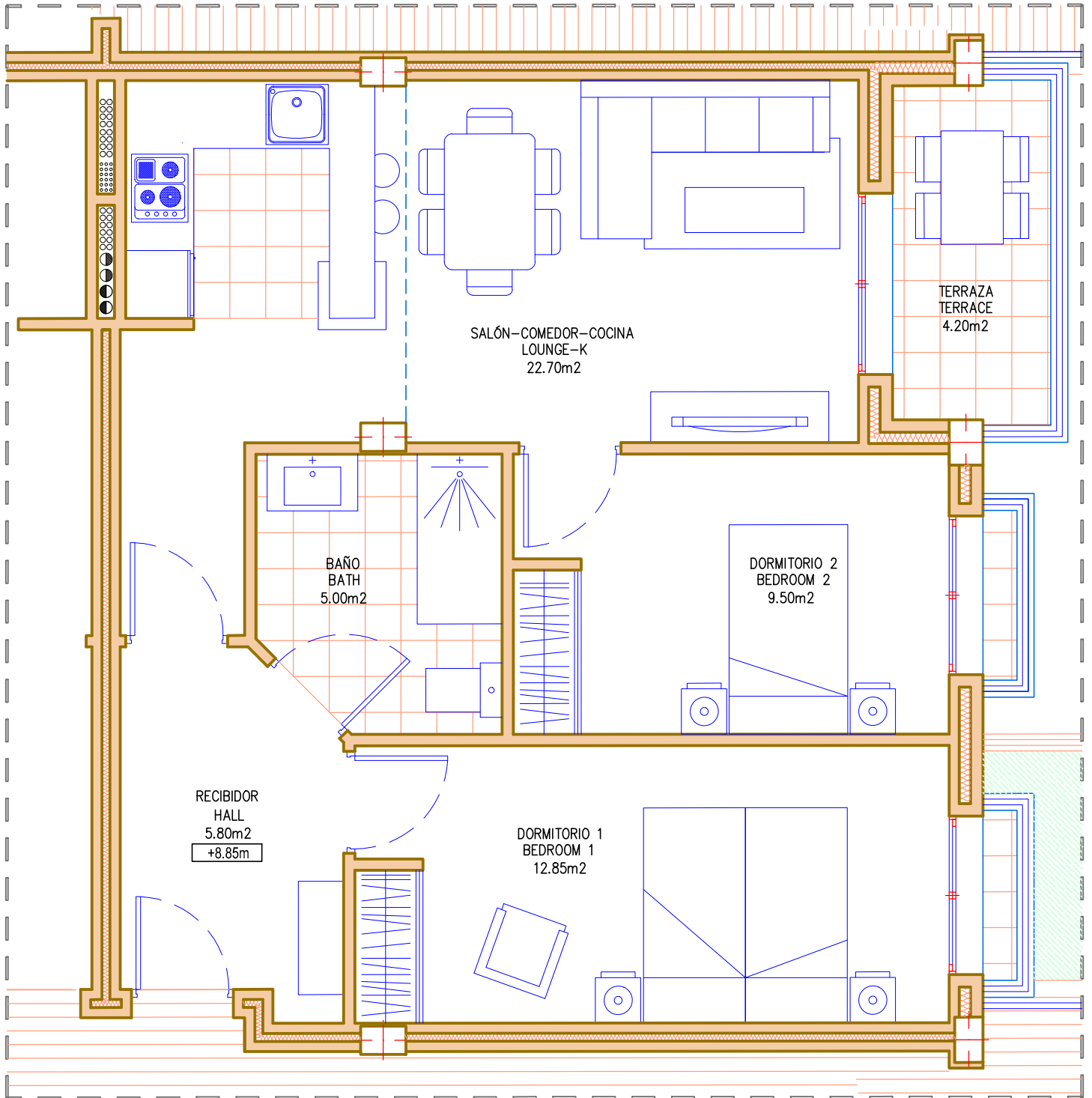
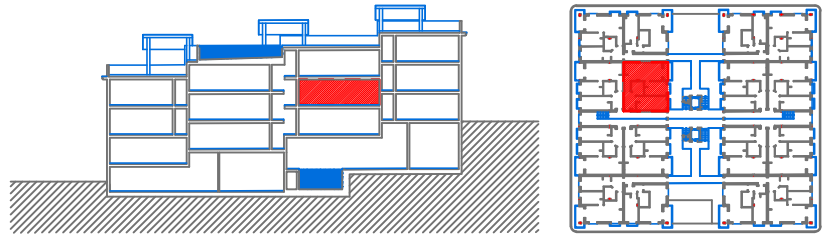


Escala\_ 1/50



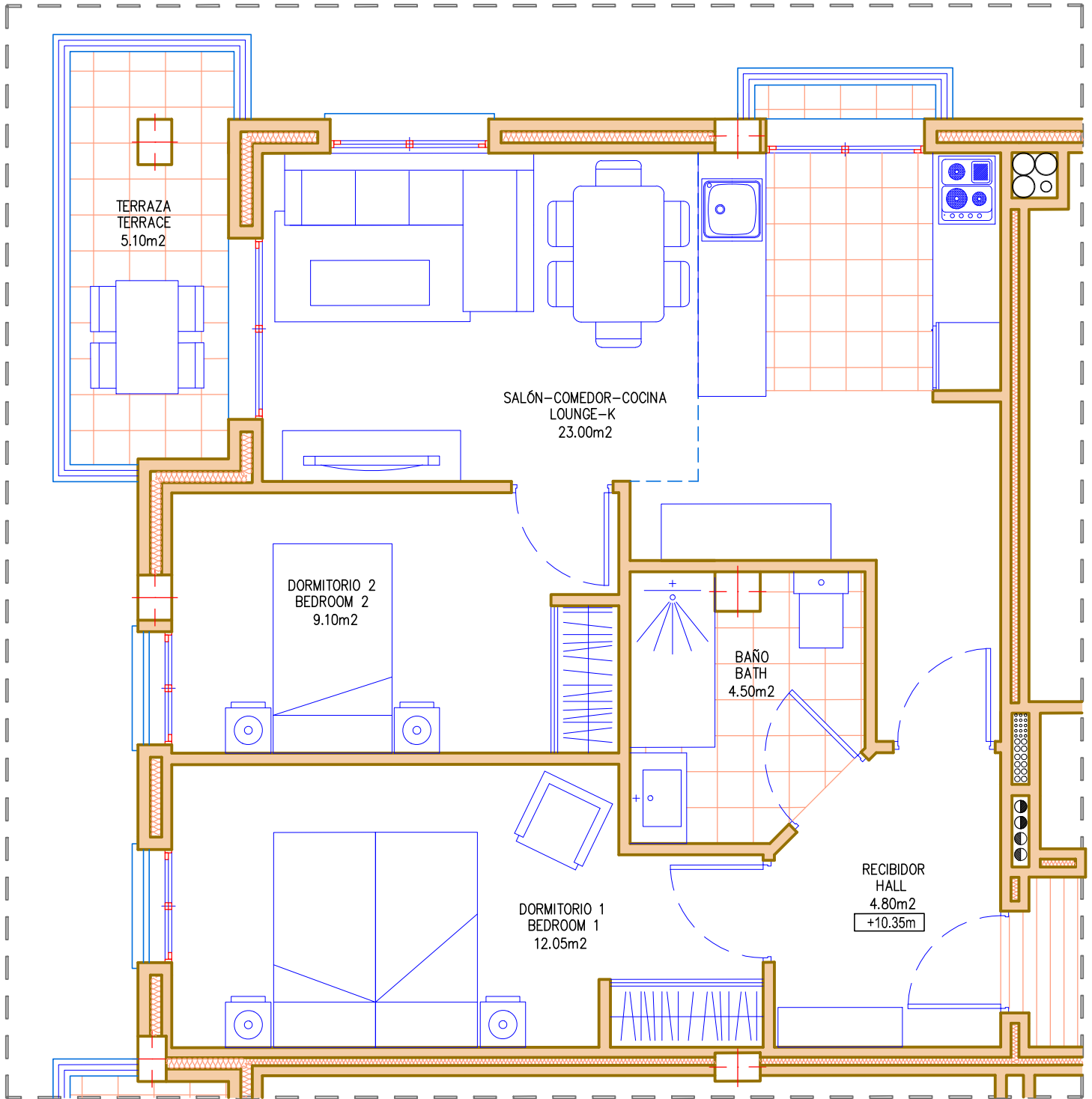
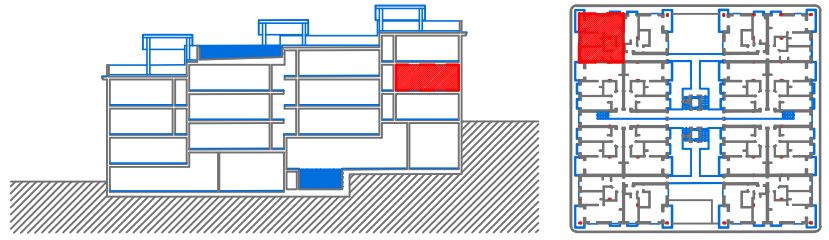
# APATAMENTO N° 26

SUP. CONSTRUIDA\_ 68.00m<sup>2</sup>  
SUP. UTIL\_ 55.85m<sup>2</sup>



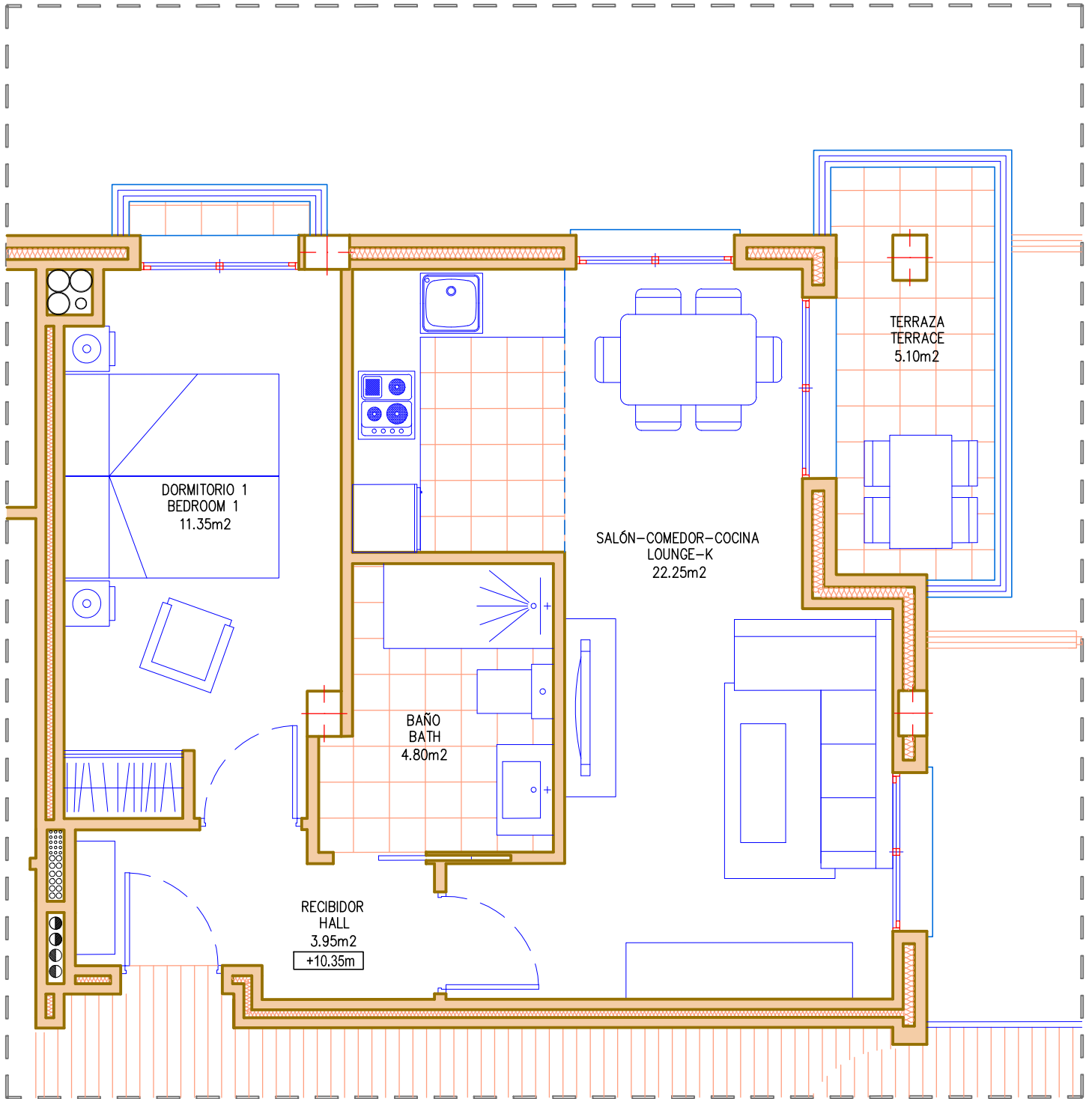
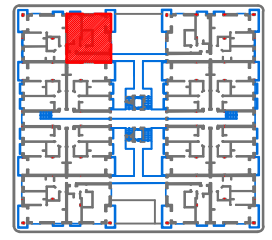
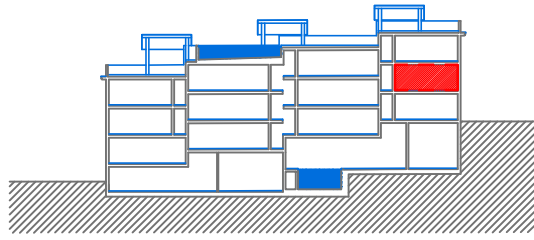
# APATAMENTO N° 27

SUP. CONSTRUIDA\_ 64.80m<sup>2</sup>  
SUP. UTIL\_ 53.45m<sup>2</sup>



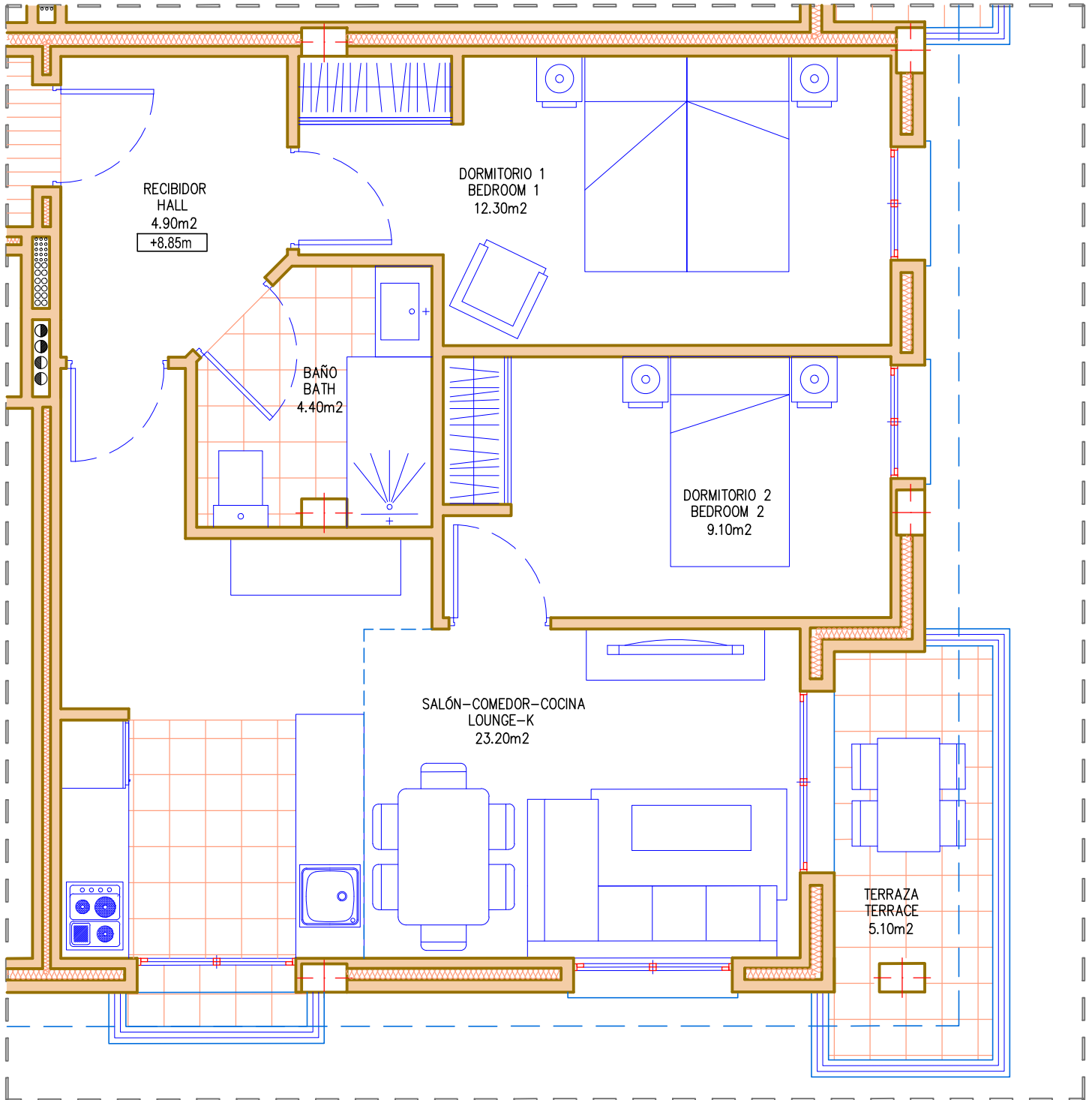
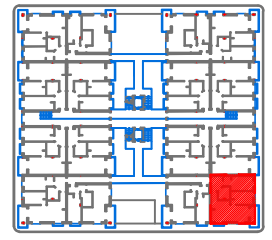
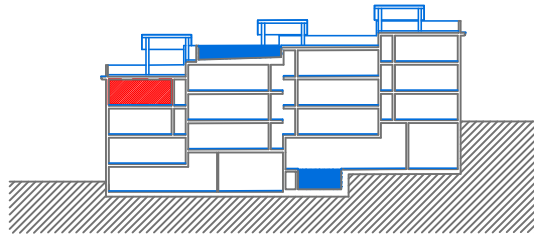
# APATAMENTO N° 28

SUP. CONSTRUIDA\_ 53.70m<sup>2</sup>  
SUP. UTIL\_ 42.35m<sup>2</sup>



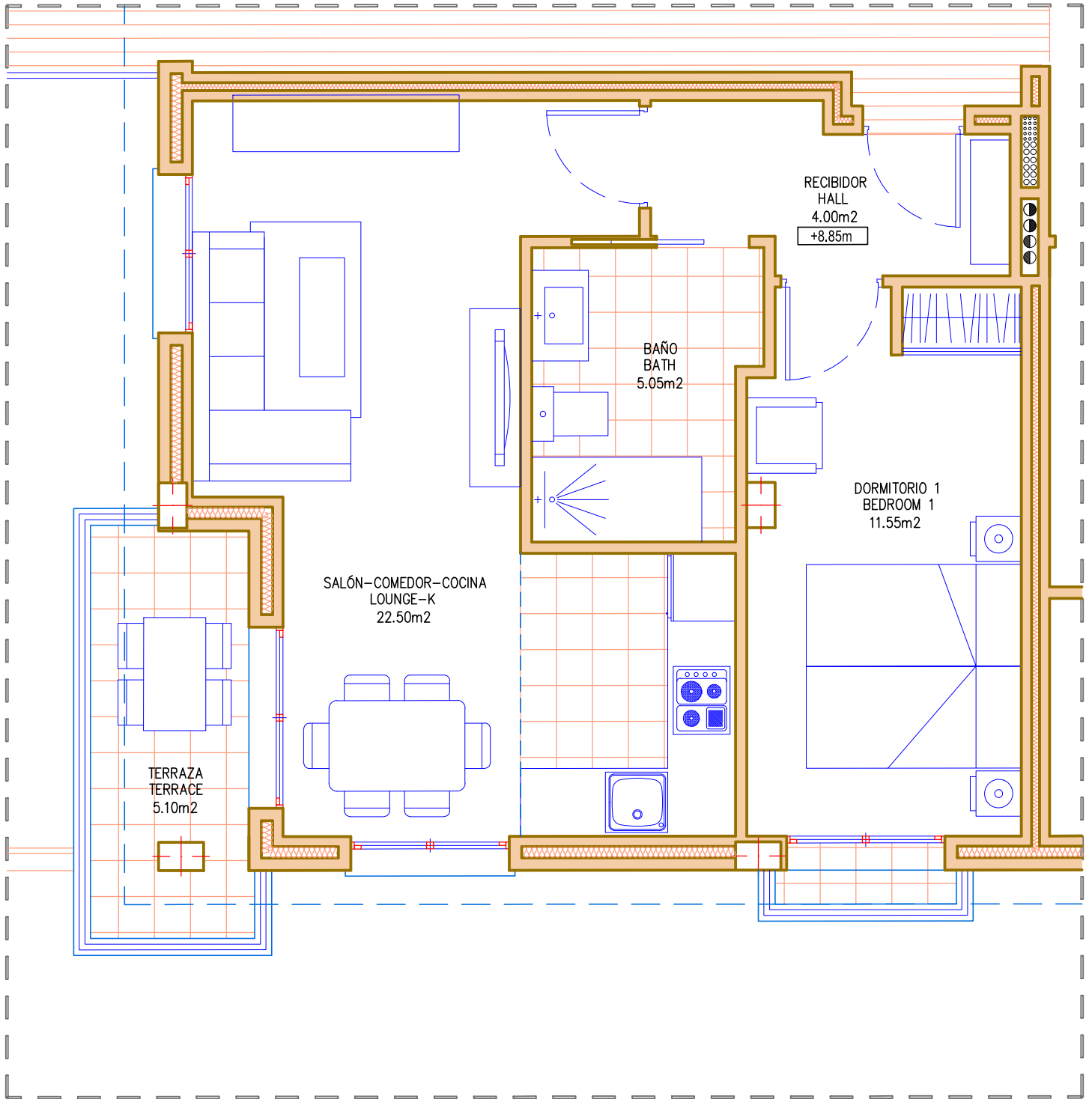
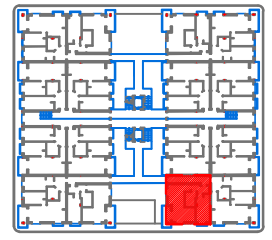
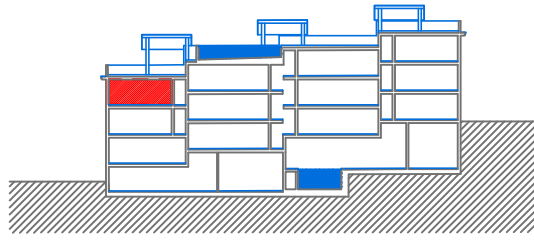
# APATAMENTO N° 29

SUP. CONSTRUIDA\_ 65.30m<sup>2</sup>  
SUP. UTIL\_ 53.80m<sup>2</sup>



# APATAMENTO N° 30

SUP. CONSTRUIDA\_ 54.05m<sup>2</sup>  
SUP. UTIL\_ 43.10m<sup>2</sup>

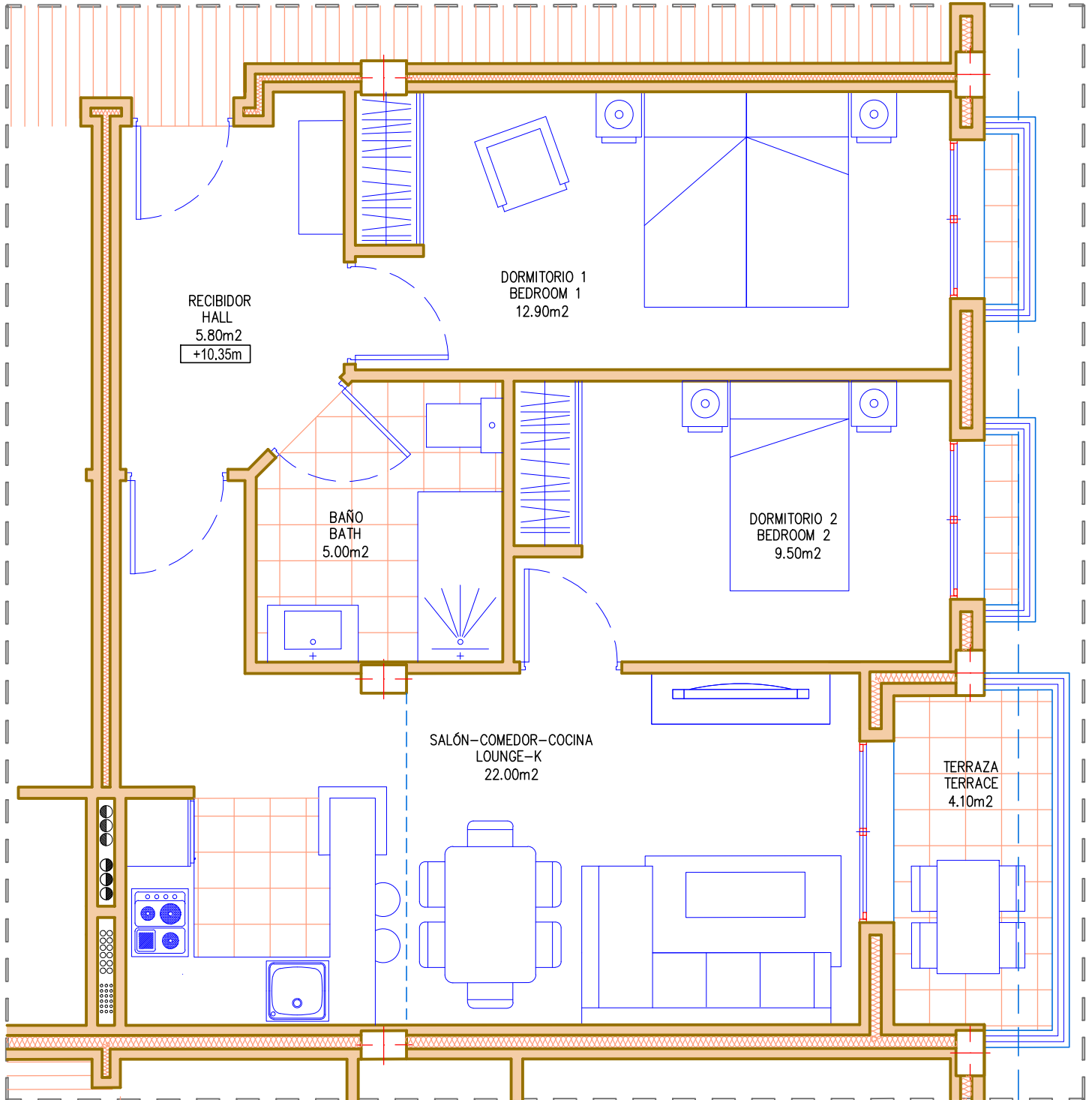
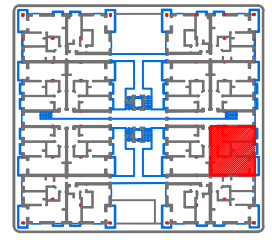
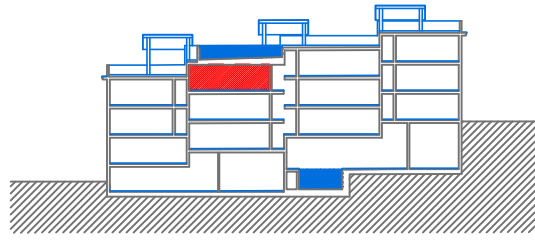


# APATAMENTO Nº 31

SUP. CONSTRUIDA\_ 66.50m<sup>2</sup>  
SUP. UTIL\_ 55.15m<sup>2</sup>



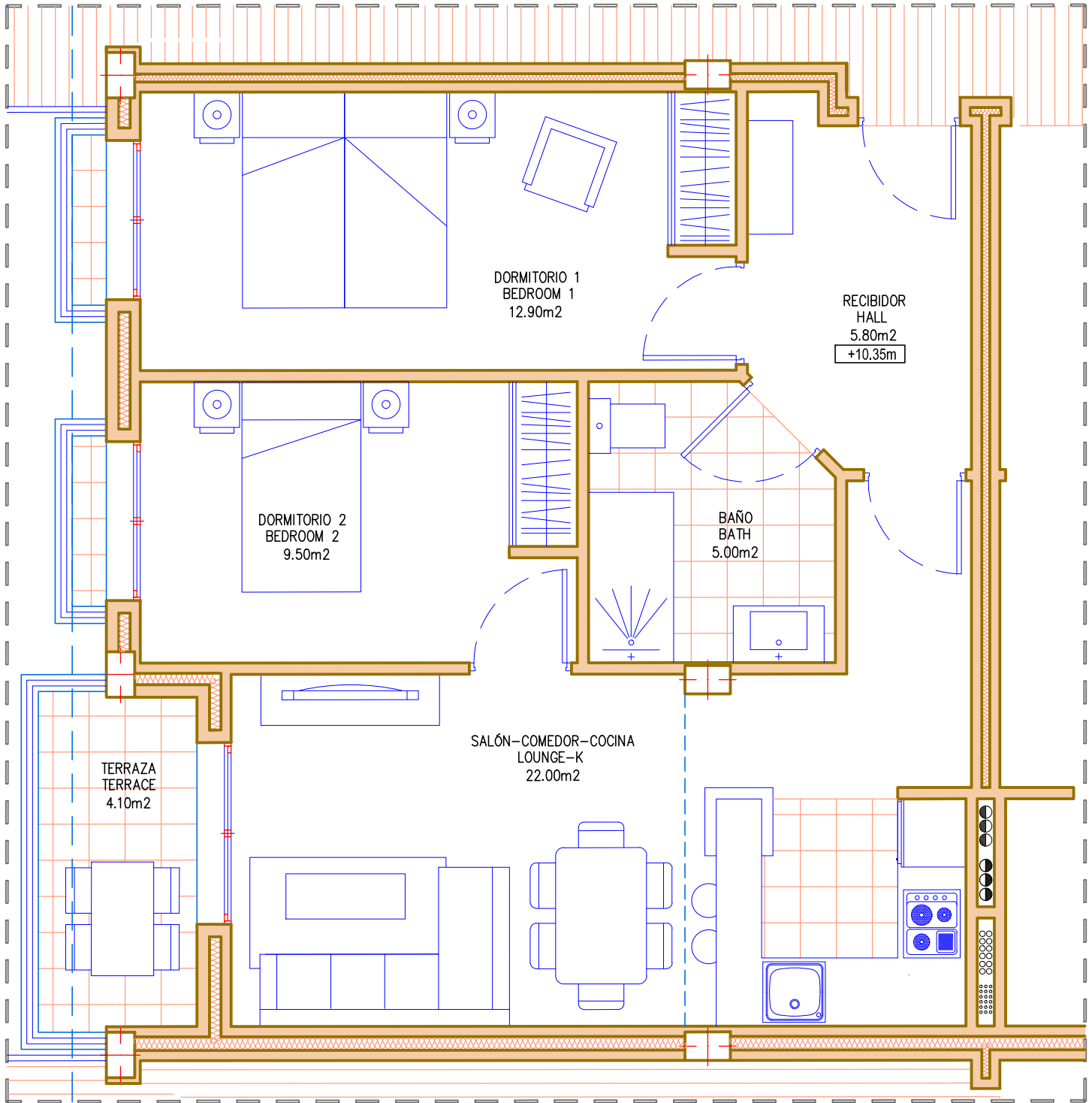
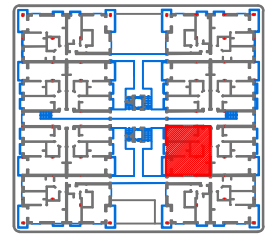
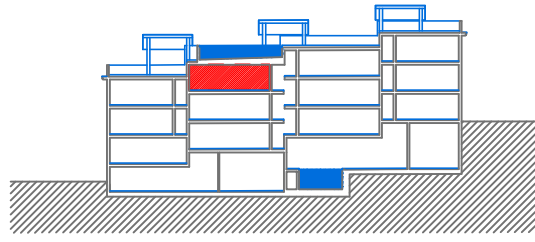
Escala\_ 1/50





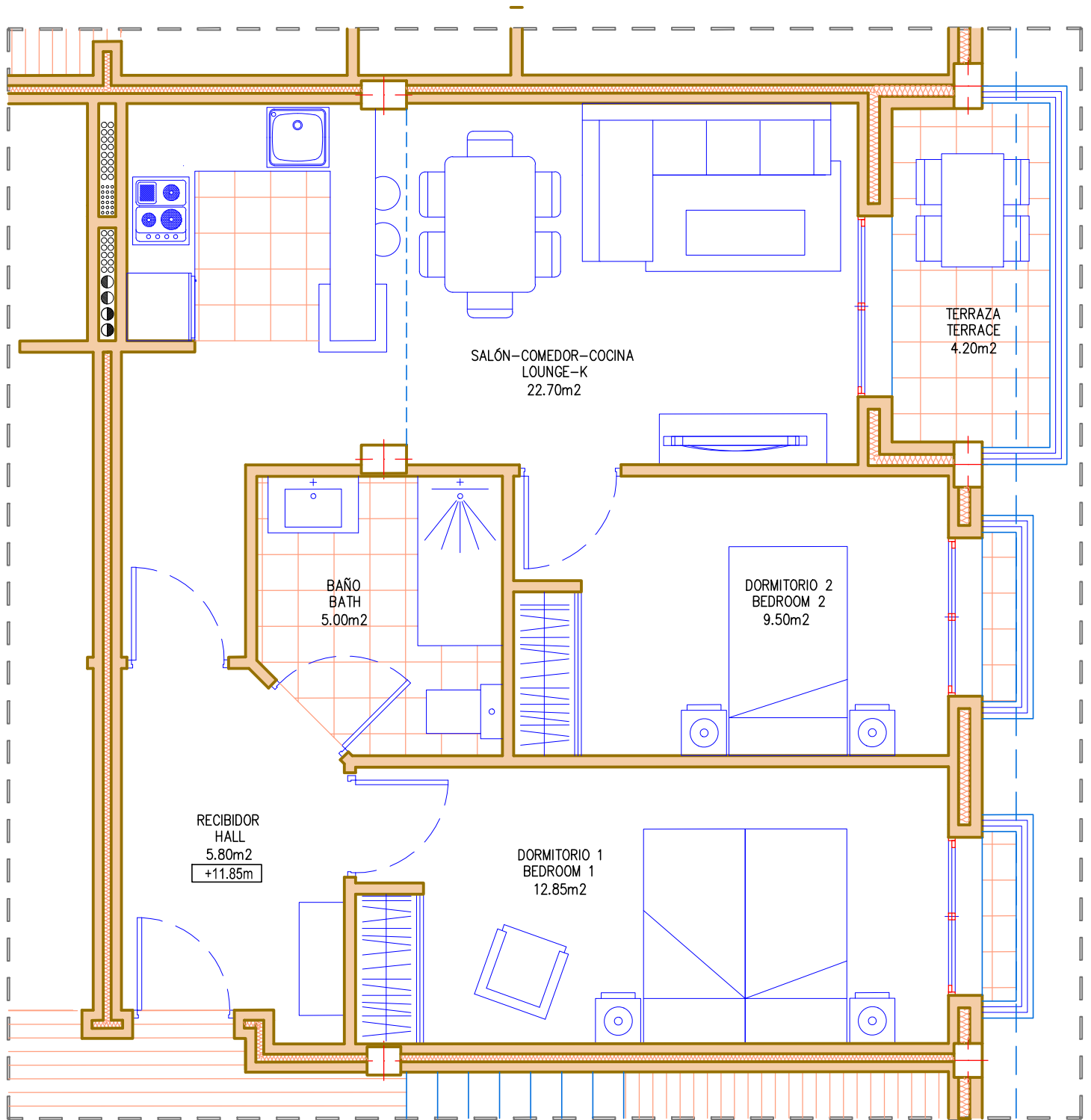
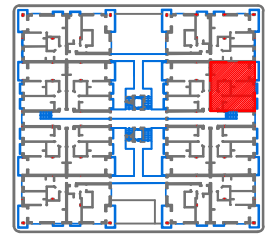
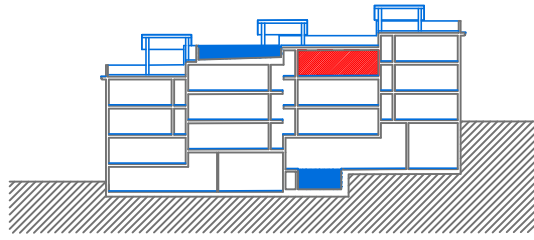
# APATAMENTO N° 32

SUP. CONSTRUIDA\_ 65.50m<sup>2</sup>  
SUP. UTIL\_ 55.15m<sup>2</sup>



# APATAMENTO N° 33

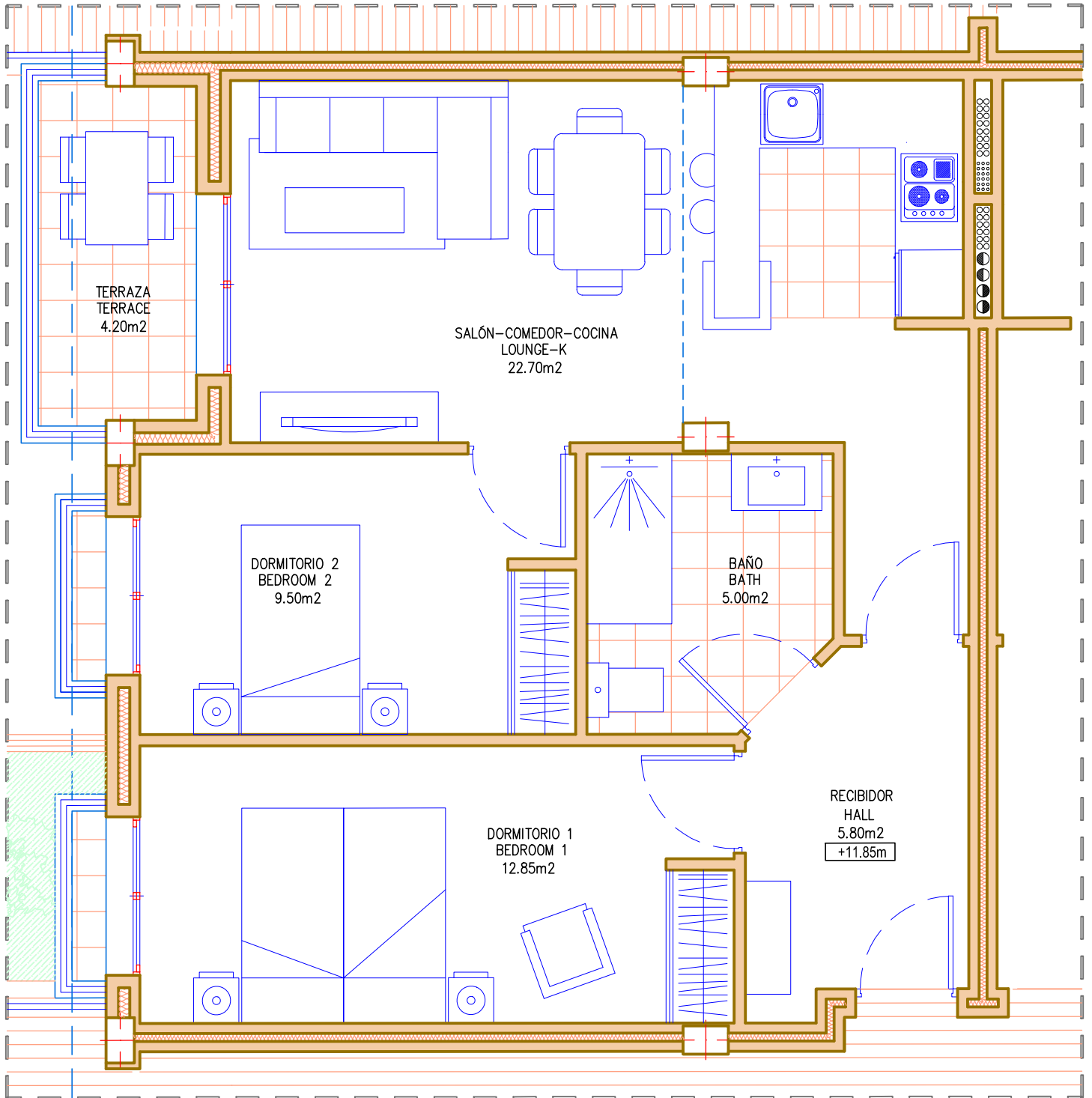
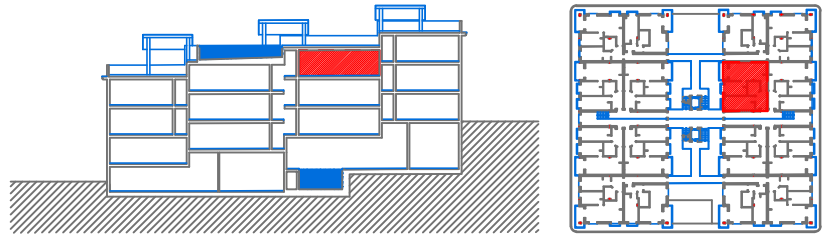
SUP. CONSTRUIDA\_ 67.05m<sup>2</sup>  
SUP. UTIL\_ 55.85m<sup>2</sup>



# APATAMENTO N° 34

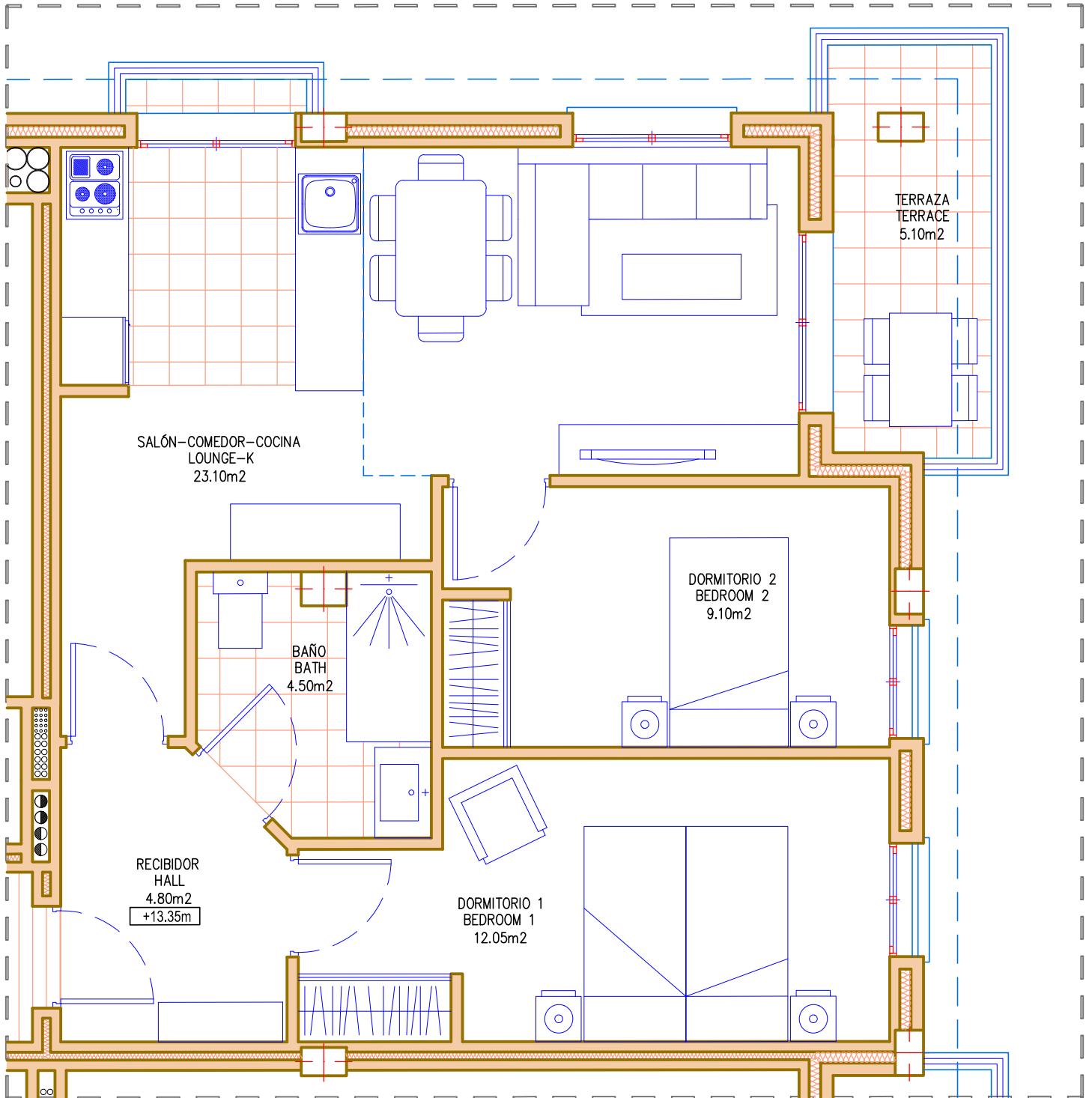
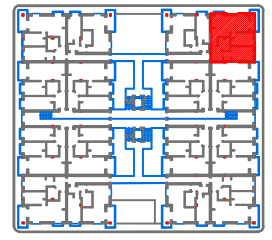
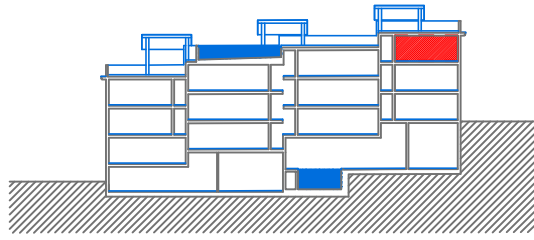
SUP. CONSTRUIDA\_ 68.00m<sup>2</sup>  
SUP. UTIL\_ 55.85m<sup>2</sup>

Escala\_ 1/50



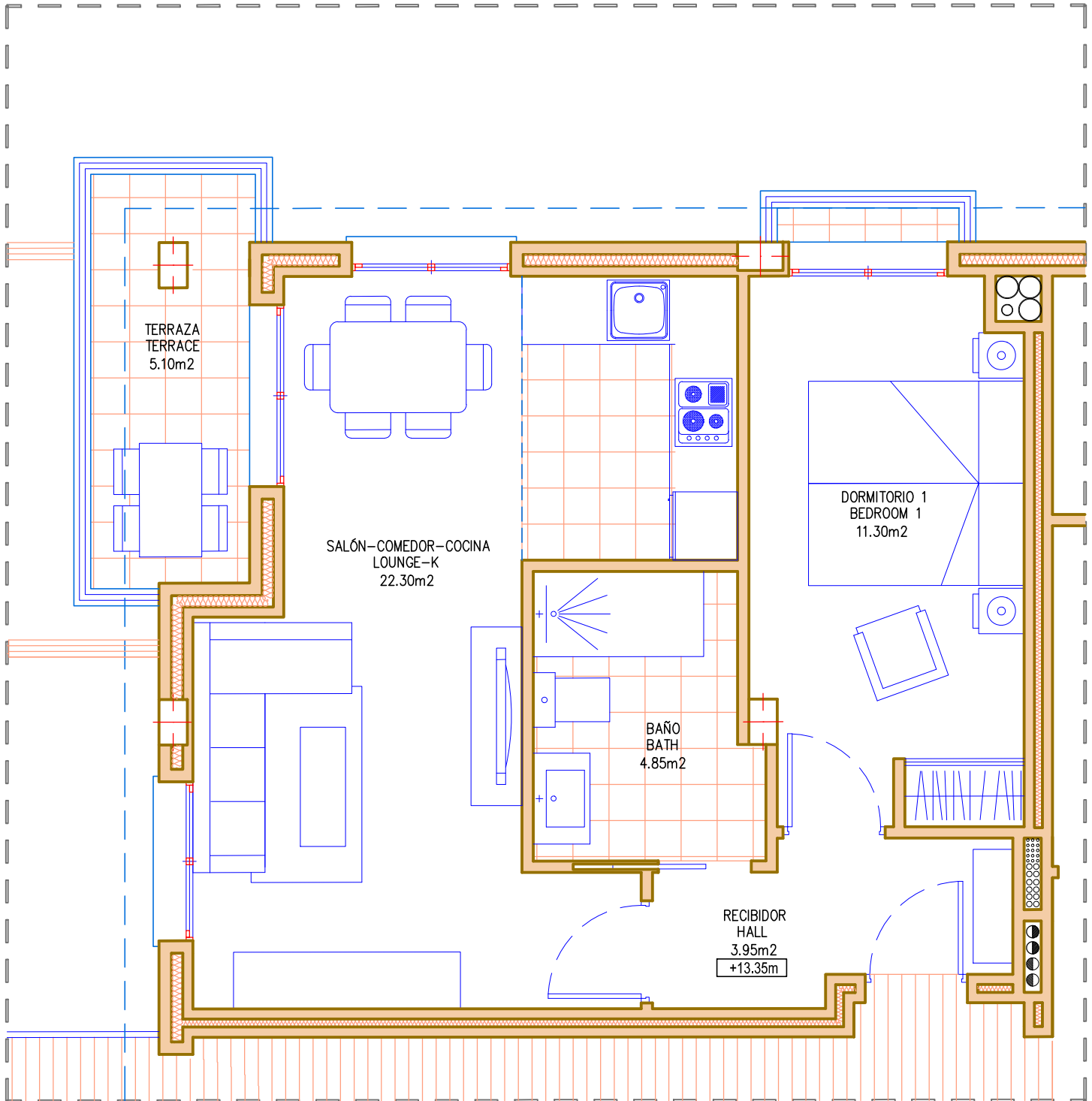
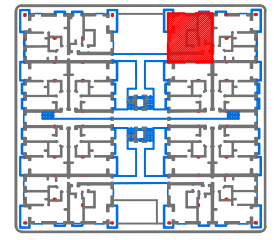
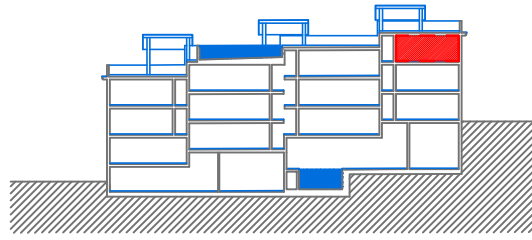
# APATAMENTO N° 35

SUP. CONSTRUIDA\_ 64.80m<sup>2</sup>  
SUP. UTIL\_ 53.55m<sup>2</sup>



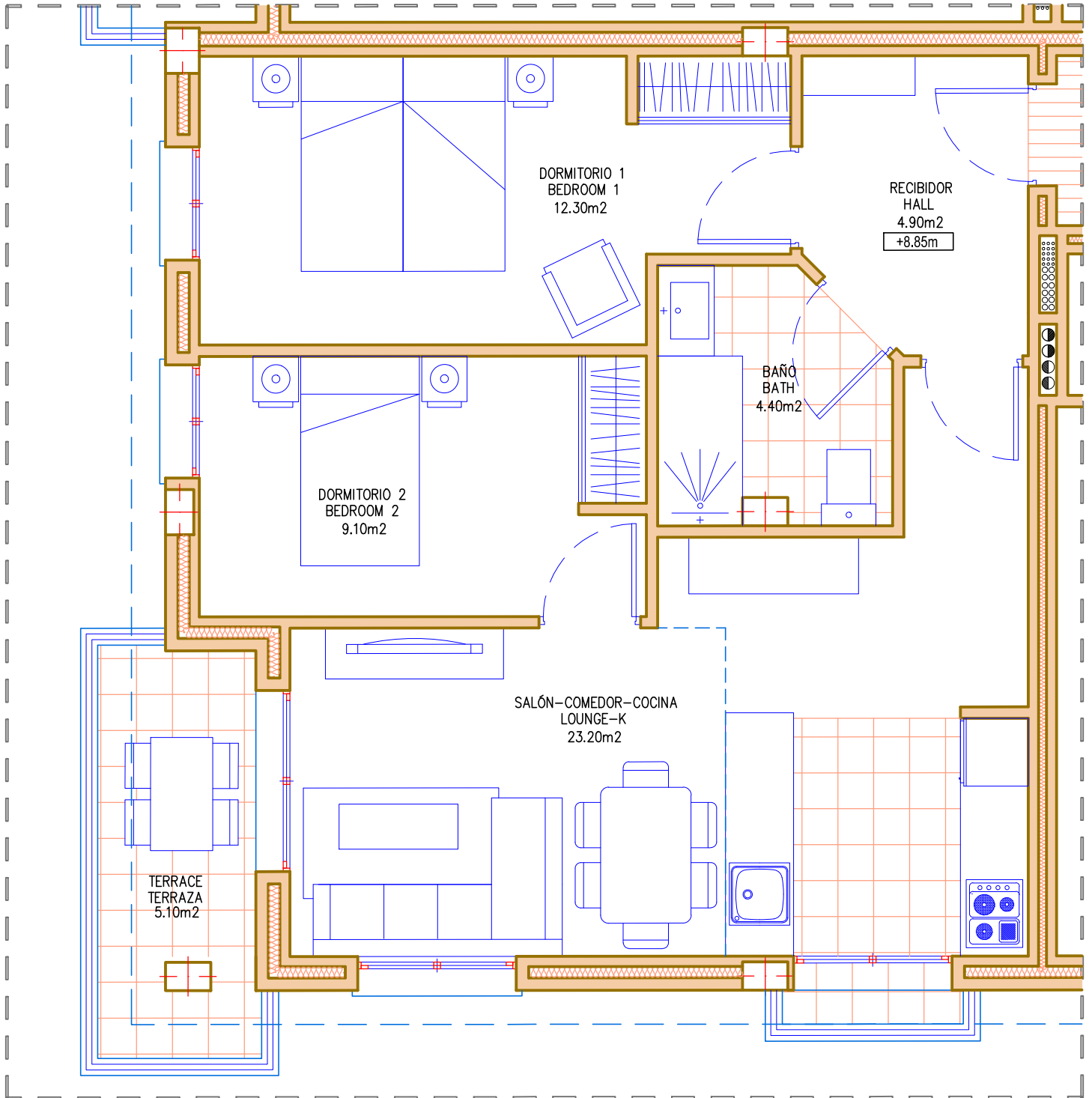
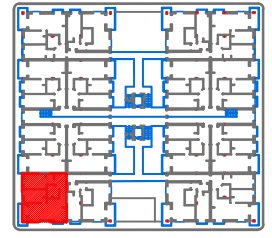
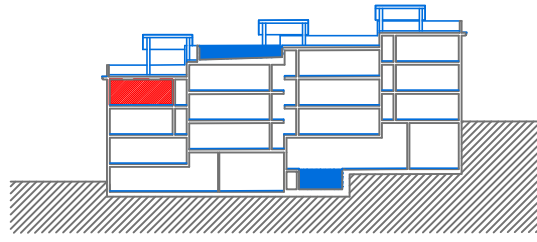
# APATAMENTO N° 36

SUP. CONSTRUIDA\_ 57.70m<sup>2</sup>  
SUP. UTIL\_ 42.40m<sup>2</sup>



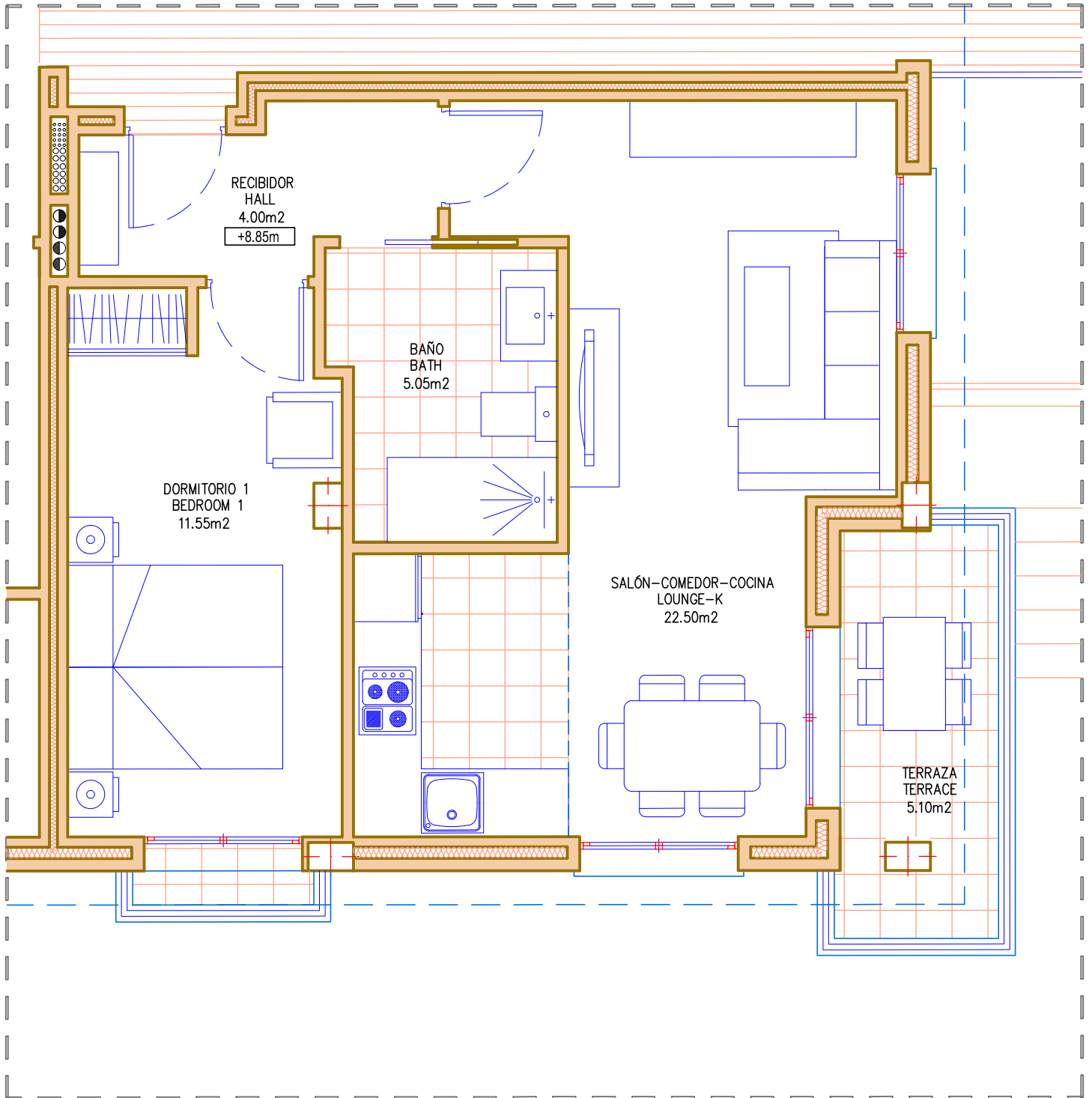
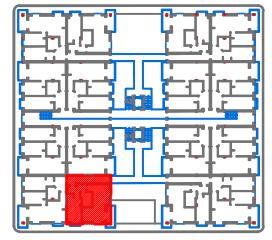
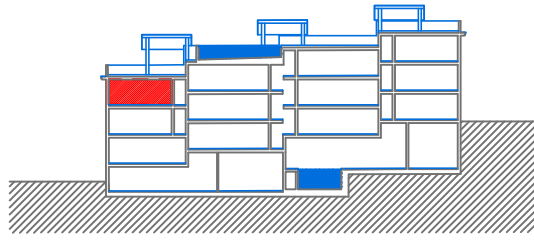
# APATAMENTO N° 37

SUP. CONSTRUIDA\_ 65.30m<sup>2</sup>  
SUP. UTIL\_ 53.90m<sup>2</sup>



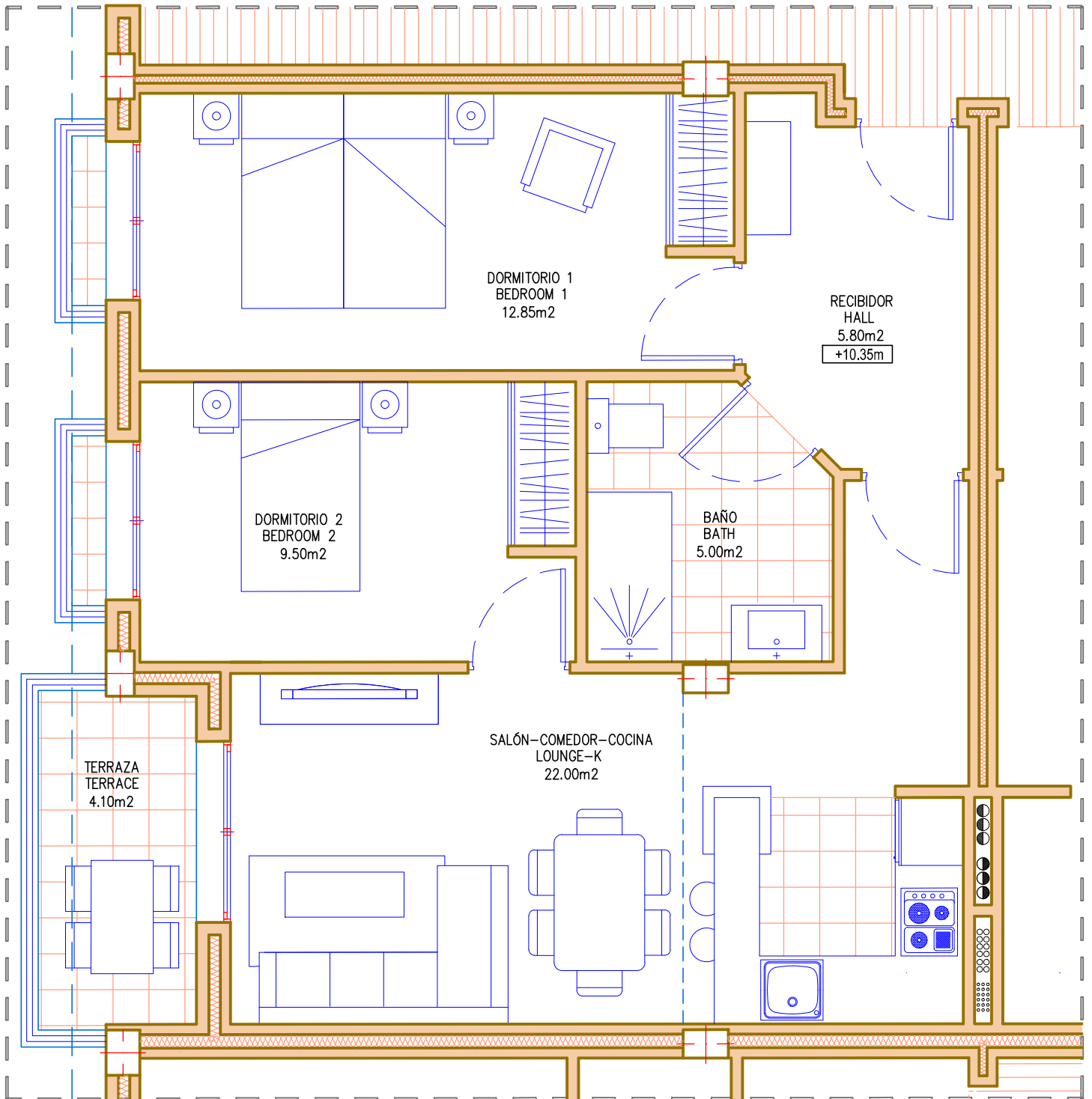
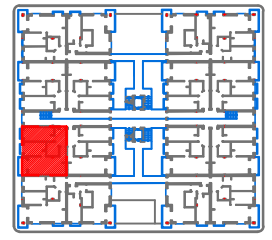
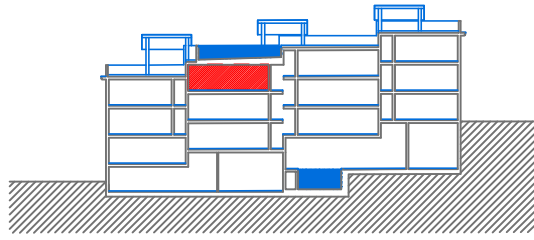
# APATAMENTO N° 38

SUP. CONSTRUIDA\_ 54.40m<sup>2</sup>  
SUP. UTIL\_ 43.10m<sup>2</sup>



# APATAMENTO N° 39

SUP. CONSTRUIDA\_ 67.05m<sup>2</sup>  
SUP. UTIL\_ 55.85m<sup>2</sup>



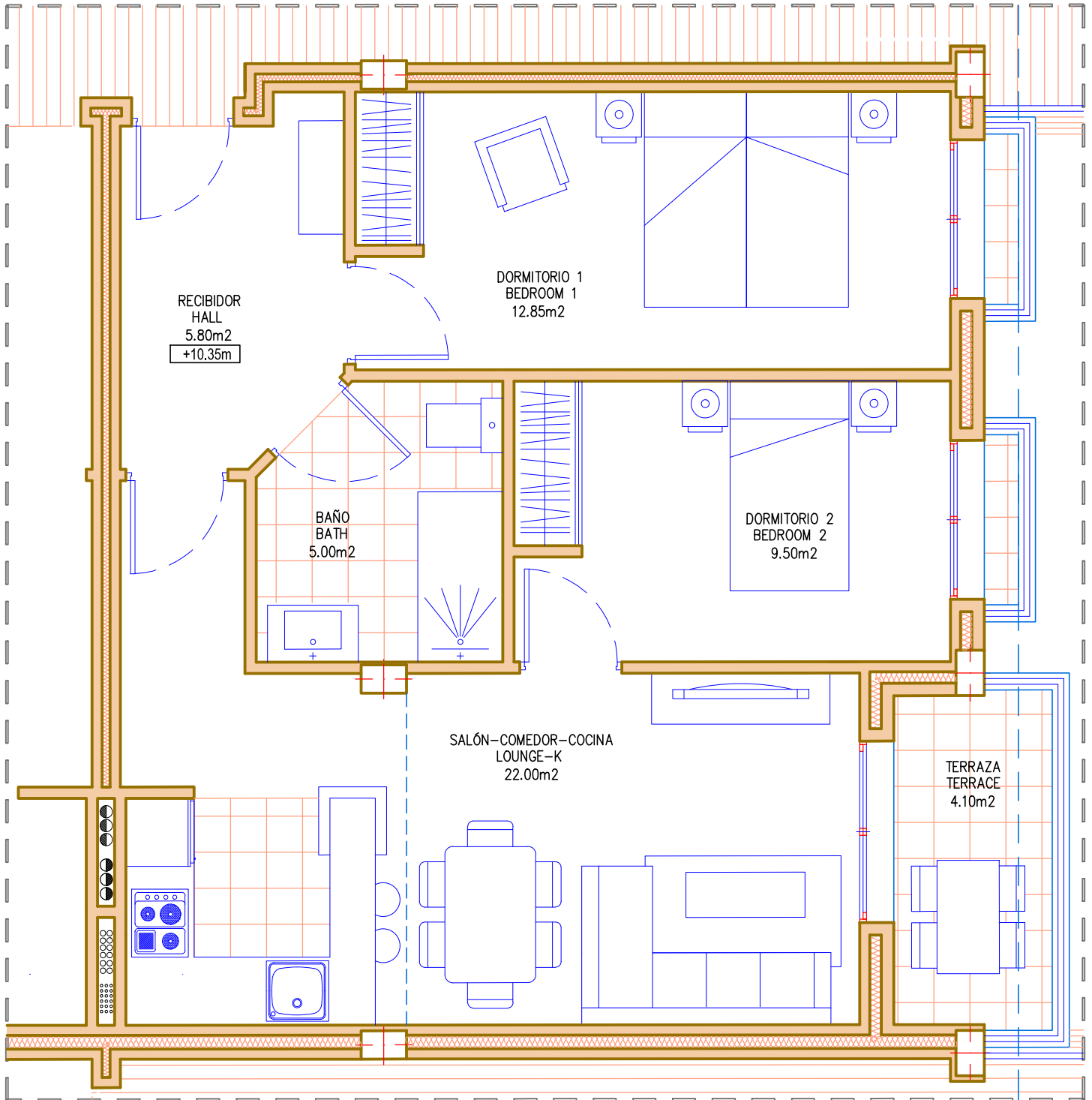
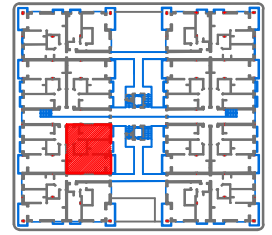
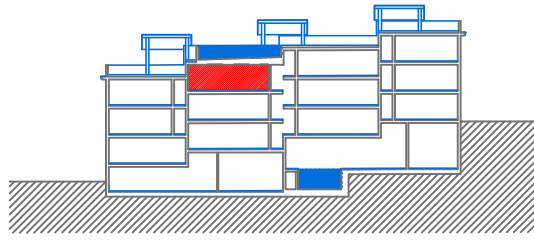


# APATAMENTO N° 40

SUP. CONSTRUIDA\_ 67.60m<sup>2</sup>  
SUP. UTIL\_ 55.15m<sup>2</sup>

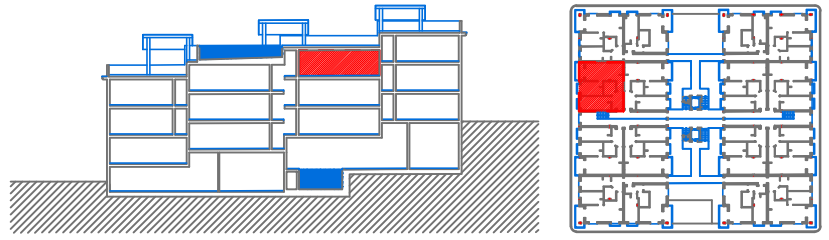


Escala\_ 1/50

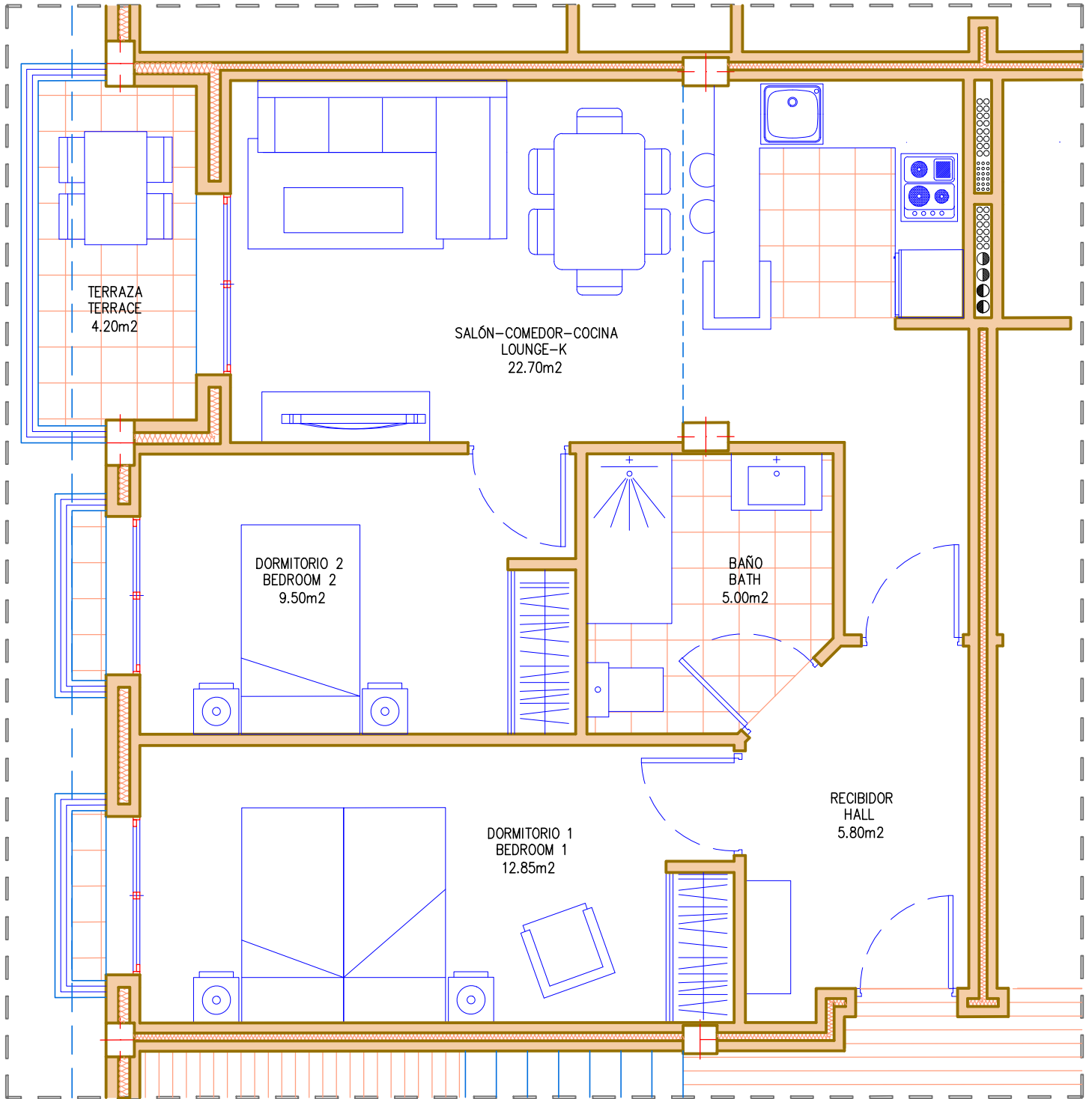


# APATAMENTO N° 41

SUP. CONSTRUIDA\_ 67.05m<sup>2</sup>  
SUP. UTIL\_ 55.85m<sup>2</sup>

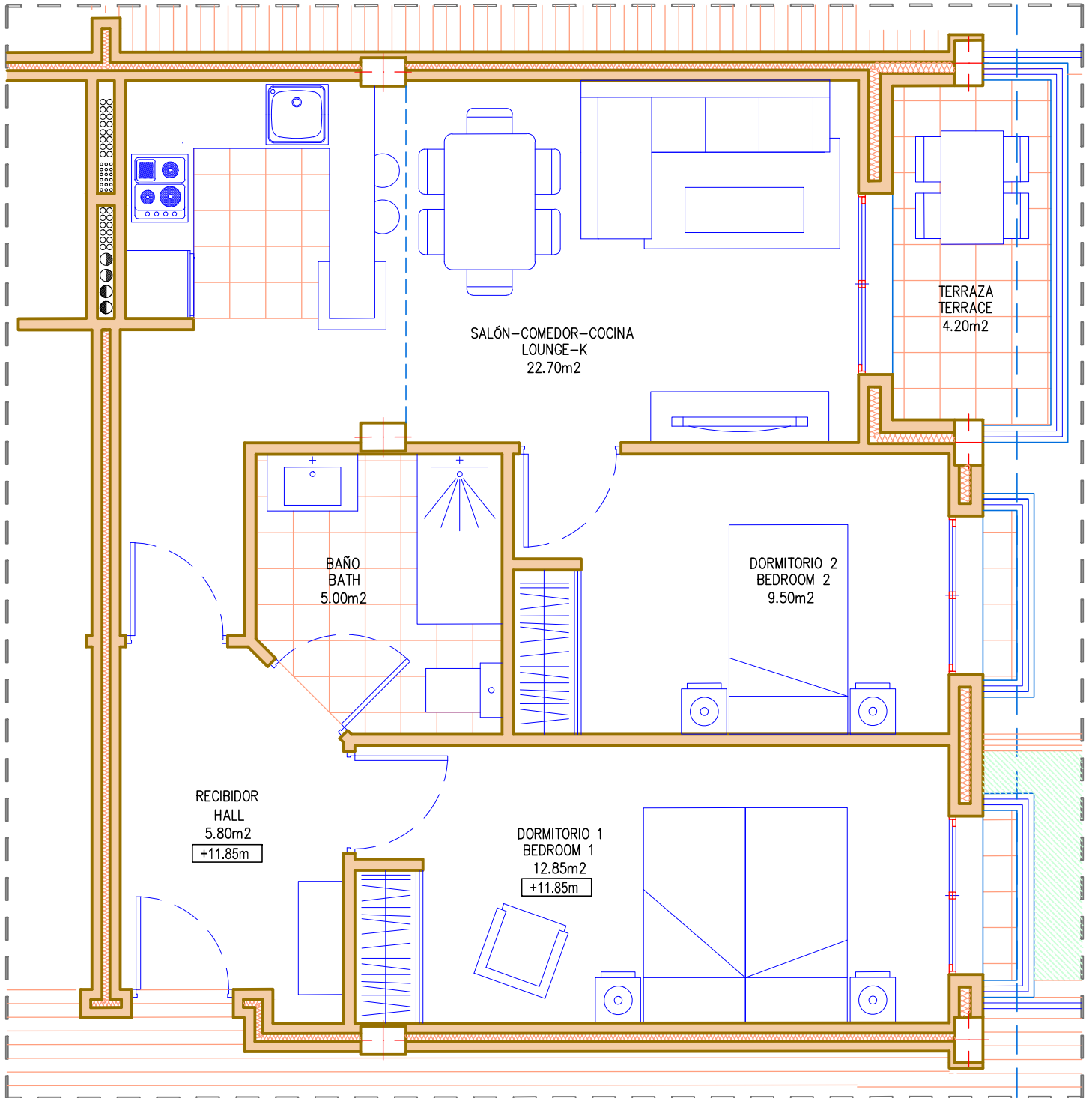
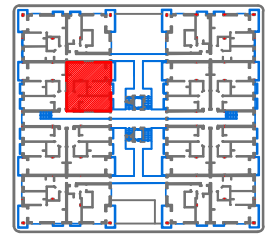
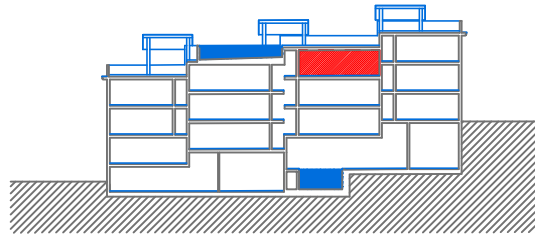


Escala\_ 1/50



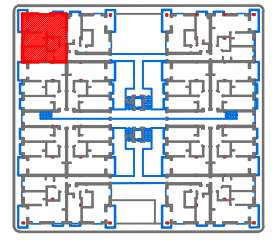
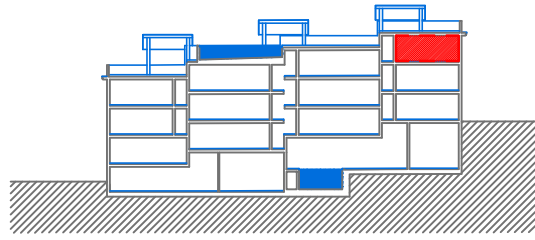
# APATAMENTO N° 42

SUP. CONSTRUIDA\_ 68.00m<sup>2</sup>  
SUP. UTIL\_ 55.85m<sup>2</sup>

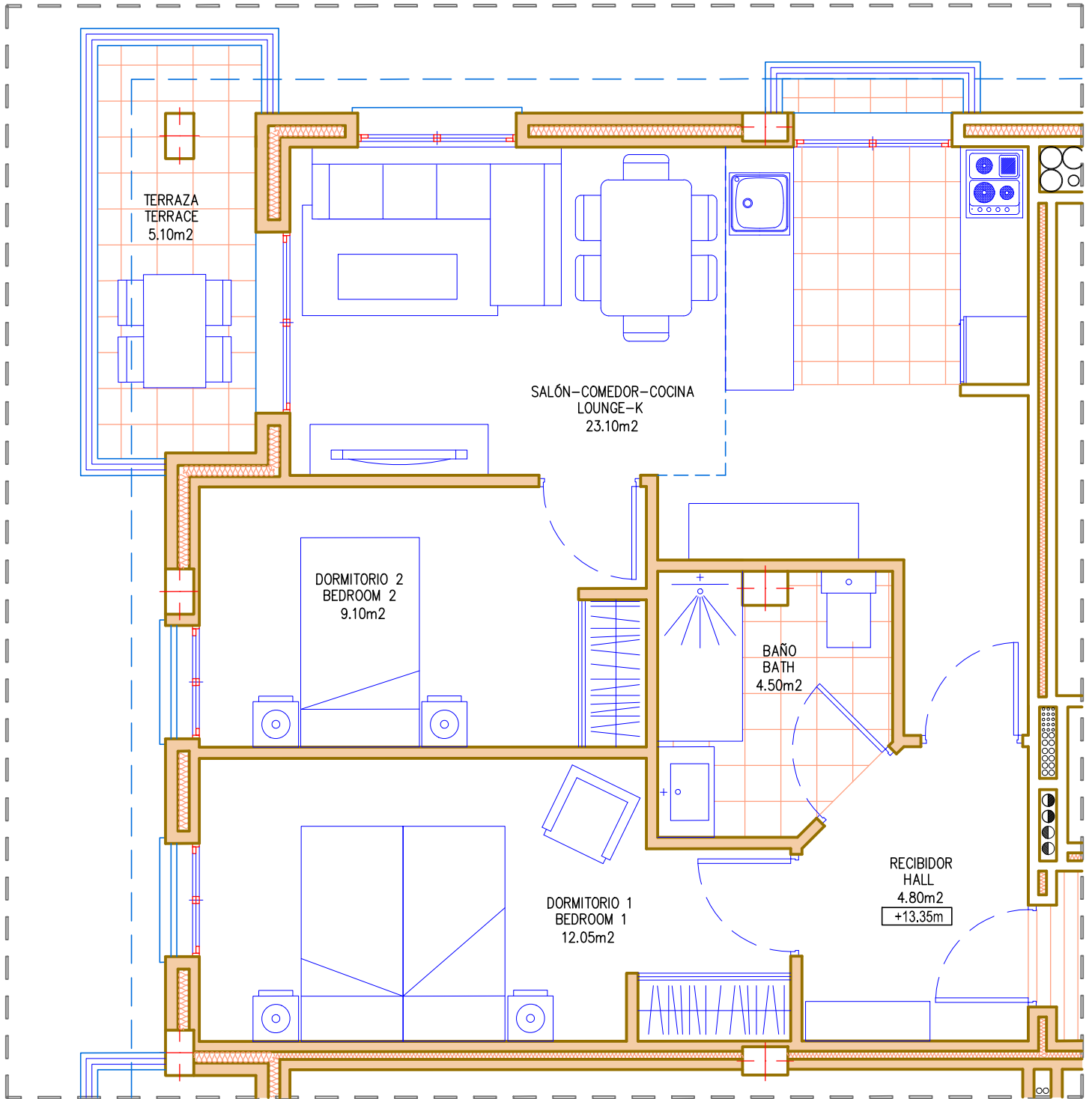


# APATAMENTO N° 43

SUP. CONSTRUIDA\_ 64.80m<sup>2</sup>  
SUP. UTIL\_ 53.55m<sup>2</sup>



Escala\_ 1/50



# APATAMENTO N° 44

SUP. CONSTRUIDA\_ 53.70m<sup>2</sup>  
SUP. UTIL\_ 42.40m<sup>2</sup>

